



Safe Routes for Seniors Pilot Neighborhood Plan

DOWNTOWN



LADOT

**Safe
Routes
for Seniors**



Acknowledgements

Council District 1

Council District 5

Council District 8

Council District 10

Council District 14

Council District 15

**Los Angeles Department
of Aging**

Prepared by

**TOOLE
DESIGN**

Information contained in this document is for planning purposes and should not be used for final design of any project. All results, recommendations, concept drawings, cost opinions, and commentary contained herein are based on limited data and information and on existing conditions that are subject to change.

Community-Based Organization Advisory Committee Members

Yolanda Breland, LA Mission

Henriëtte Brouwers, LA Poverty Department

Yasue Katsuragi, Little Tokyo Service Center

Alice Corona, The People's Concern

Lindsey Ridler, Chrysalis

Pastor Blue, Blue Hollywood Street Sanctuary

Amelia Rayno, Blue Hollywood Street Sanctuary



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Chapter 1

Executive Summary

Why Safe Routes for Seniors?

Safe Routes for Seniors (SRFS) is a proactive response to the mobility and safety needs of older adults in urban environments. The needs of older adults are not typically reflected in the way sidewalks, bike lanes, and roadway crossings are designed and built. When hit by a vehicle traveling 20 mph, pedestrians aged 65 and older face a fatality risk triple that of pedestrians aged 25–64.¹ A 70-year-old pedestrian struck at 20 mph experiences the same likelihood of severe injury as a 30-year-old struck at 32 mph.² Traffic safety concerns can result in older adults choosing to stay home, which increases social isolation.

In 2022, Los Angeles Department of Transportation (LADOT) initiated the SRFS pilot program to address the needs of older adults. LADOT has produced five SRFS Neighborhood Plans with infrastructure recommendations for transportation safety improvements.

These recommendations are based on needs identified by older adults who live or frequently visit each neighborhood. They are designed to significantly enhance safety and accessibility, reduce the incidence of crashes involving older adults, and improve the overall quality of life in the pilot neighborhoods. This older adult-informed initiative is especially important, as the population of older adults in Los Angeles is projected to continue to grow significantly.

Safe Routes for Seniors not only addresses immediate concerns for older adults, but it also sets a precedent for future urban planning that centers the stated needs of older adults in order to support their overall well-being.



¹ Leaf, W. A. & Preusser, D. F. (1999). Literature Review on Vehicle Travel Speeds and Pedestrian Injuries (DOT HS 809 021). Washington, DC: United States Department of Transportation. NHTSA.

² Tefft, B. C. (2013) Impact speed and a pedestrian's risk of severe injury or death, *Accident Analysis & Prevention*, Volume 50, 871-878.

Neighborhood Mobility Opportunities and Challenges



Older adults who participated in project surveys stated they primarily move around in Downtown by **walking or using a mobility device** like a wheelchair (80 percent).



Key transportation safety concerns identified through community engagement were that **people drive too fast** and **sidewalks are missing or in poor condition**.



Reported transportation safety issues were concentrated in **Skid Row and Little Tokyo**.



Collisions in the neighborhood that involved older adults walking and biking mainly occurred at intersections and during daytime hours, with 17 percent resulting in severe injuries or fatalities.

Pilot Neighborhood Plan: Downtown

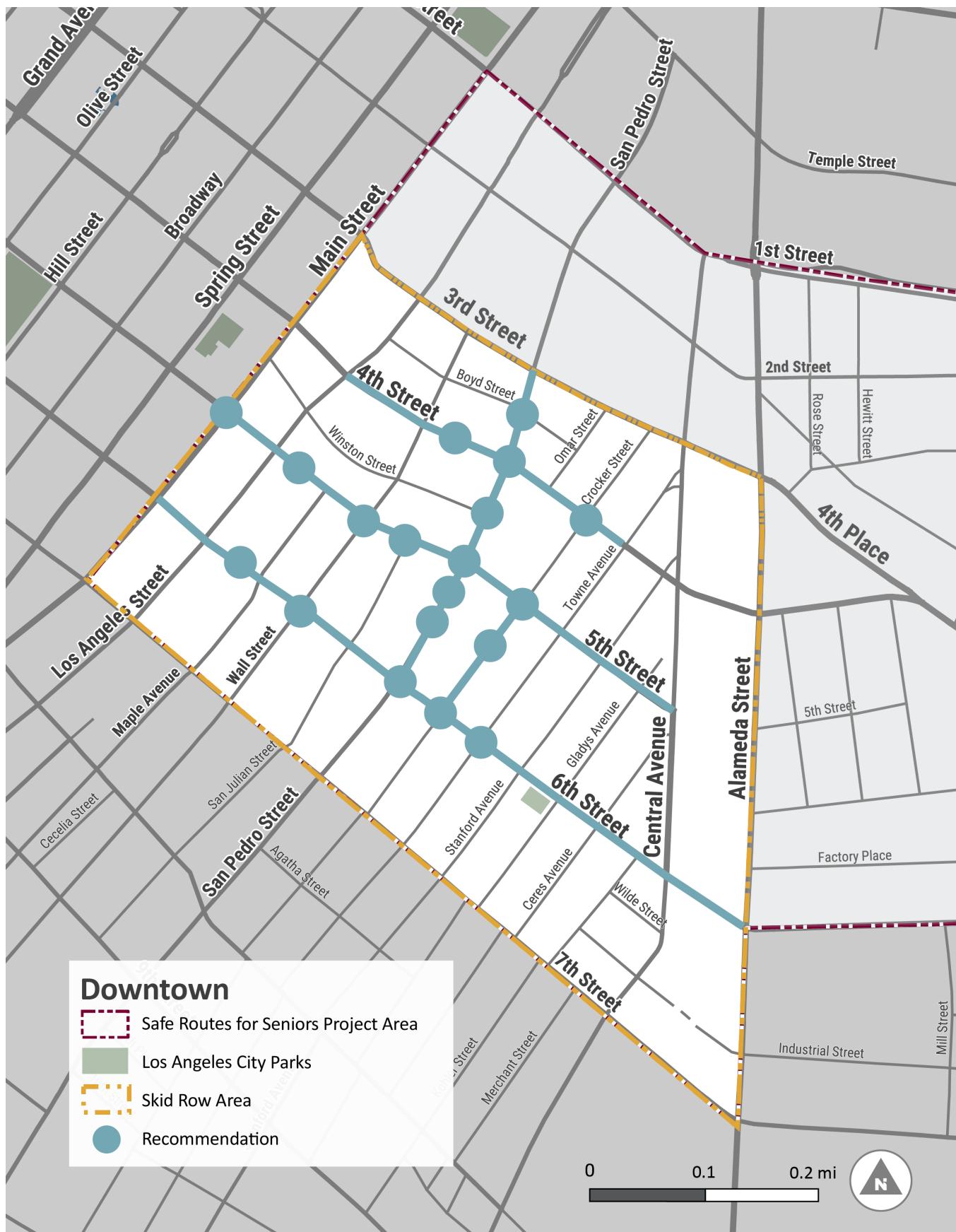
The Downtown neighborhood is defined for this pilot as the area bordered by 1st Street, 7th Street/6th Street, Main Street, and the Los Angeles River. However, a review of existing conditions revealed that community destinations, resources and housing for older adults, and High-Injury Network streets were concentrated in Skid Row and Little Tokyo (see Chapter 3), and the project study area was refined to focus on those two neighborhoods.

LADOT reviewed existing conditions and engaged deeply with the community by conducting surveys, workshops, and tours, and collaborating with a Community-Based Organization Advisory Committee to understand the experiences and needs of older adults in the project area.

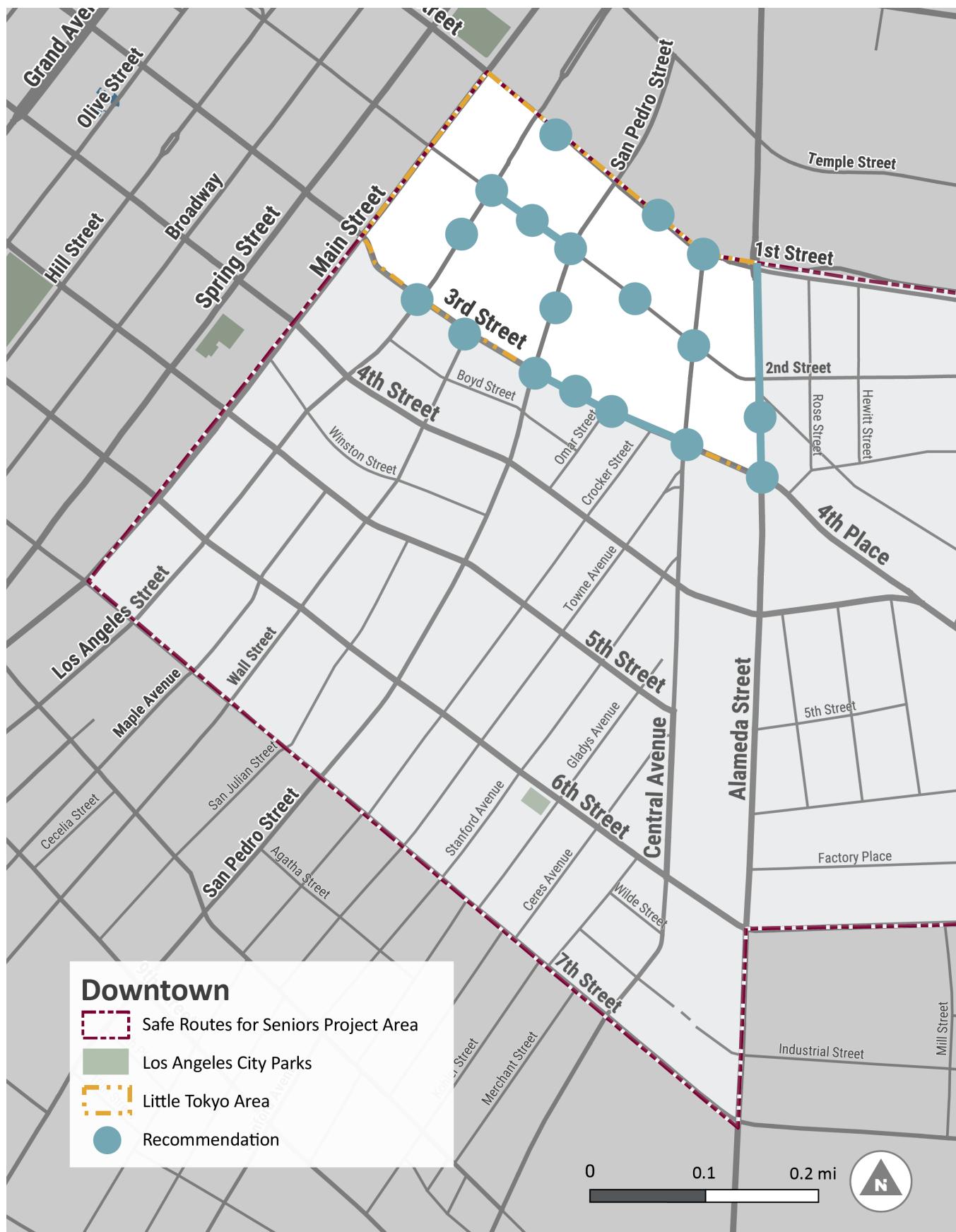
Recommendations

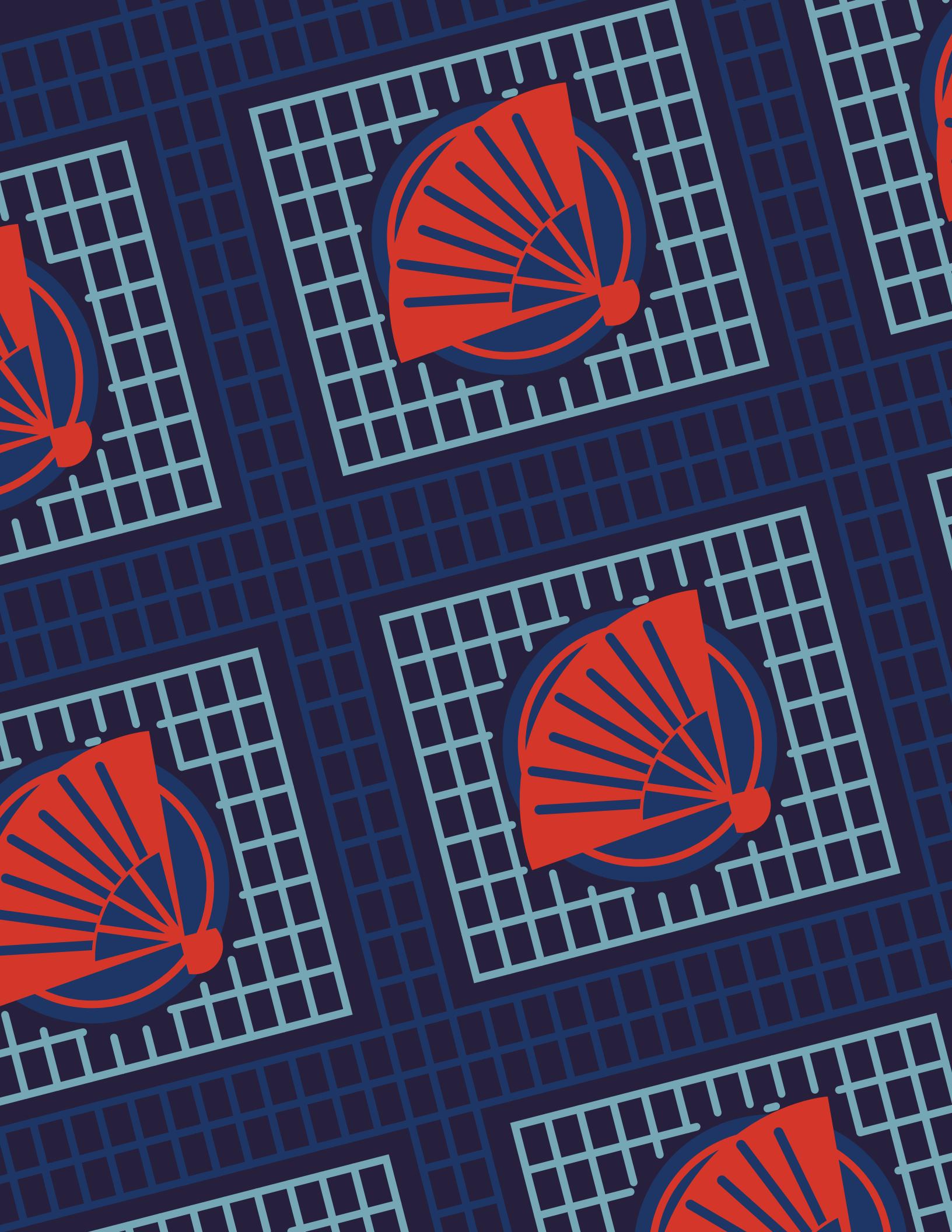
Based on feedback, recommendations in the Downtown neighborhood focus on roads within Skid Row and Little Tokyo and include crossing improvements that address long crossing distances across arterials, calm traffic at intersections, and shorten the distances between crossings by adding new midblock improvements (see Map 1). Recommendations also improve pedestrian comfort along major streets by adding streetlights and shade and by addressing sidewalk issues.

MAP 1 Downtown Neighborhood Recommendations - Skid Row Area



Downtown Neighborhood Recommendations - Little Tokyo Area







Chapter 2

What is Safe Routes for Seniors?

What is Safe Routes for Seniors?

The Los Angeles Department of Transportation (LADOT) created the Safe Routes for Seniors (SRFS) program to respond to traffic risks for older adults when walking in their neighborhoods. While making up 13 percent of the City's population in 2019, older adults accounted for 29 percent of traffic deaths. The City Controller predicts that one in four Angelenos will be 65 or older by 2030.³ The SRFS program proactively addresses this demographic shift and endeavors to reduce collisions that lead to deaths and severe injuries among older adults.

The program seeks to enhance safety, mobility, comfort, and social connectivity for older Angelenos by focusing on the most relevant changes identified through various community conversations and data analysis.

The Pilot Neighborhood Plans in Chinatown, Downtown, Exposition/Crenshaw, South LA, and Rancho Park were funded by Caltrans' Active Transportation Program. Plan coordination with other relevant local and regional plans and initiatives is detailed in Appendix A.

Who is an "older adult"?

The term "older adult" refers to individuals aged 65 and above. This phase of life encompasses a diverse range of abilities, needs, lifestyles, and life circumstances. The recommendations in the Plan are designed to address this diversity, serving both those who regularly integrate physical activity into their daily lives and those whose ability or interest in physically activity may be diminished.

Program Goals



Reduce isolation and improve health outcomes for older adults by enhancing access to direct social and health care services, jobs, healthy food, retail, and recreation.



Increase older adult walking and bicycling by addressing barriers including infrastructure disrepair, limited crossings, inaccessibility, and lack of shade and rest areas along travel routes.



Eliminate crashes that lead to deaths and serious injuries for older adults (those aged 65 and older) in Los Angeles.



Empower older adults to actively participate in identifying their transportation needs, desired program elements, and potential routes that would improve quality of life and establish ways to ensure their input is valued and addressed.

³ City Controller. (2018). Engaging Older Angelenos: Making L.A. the Age Friendliest City in America. <https://ladotlivablestreets-cms.org/uploads/935604672f6c414c9003431147b21f5c.pdf>

Why focus on older adults?

Older adults are affected by the design of their communities.

Older adults spend more of their time at home and in their immediate neighborhoods than younger adults.

By 2030,

one in every four Los Angeles residents will be an older adult.

Older adults are over-represented in traffic deaths.

Streets should be safe for everyone!

Improving streets for older adults means making streets safer for people of all ages.

Selecting the SRFS Pilot Neighborhoods

All neighborhoods in Los Angeles were assessed using six criteria that reflect the need for safety, mobility, and accessibility improvements for older adults. These indicators, selected by LADOT, include high rates of collisions involving older adults, larger older adult population, presence of senior centers, high pollution and social vulnerability, hotter average temperatures, and low car ownership.

Five neighborhoods that consistently scored the highest across these indicators were selected for the pilot program: Chinatown, South LA, Rancho Park, Exposition/Crenshaw, and Downtown. See Appendix B for more details on the neighborhood selection process.



High collision rates involving older adults



High older adult population



Presence of senior centers



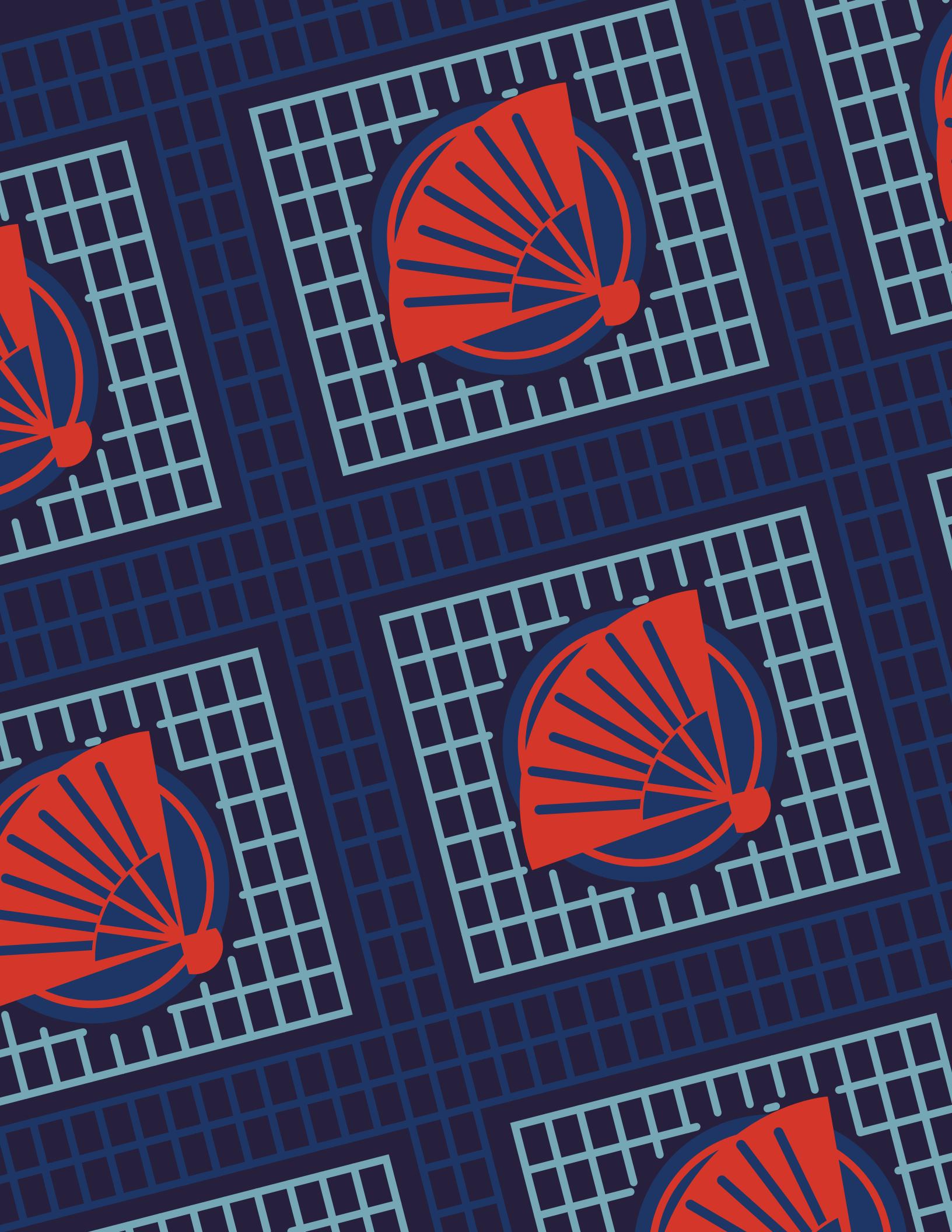
High pollution and social vulnerability



Hotter average temperatures



Low car ownership





Chapter 3

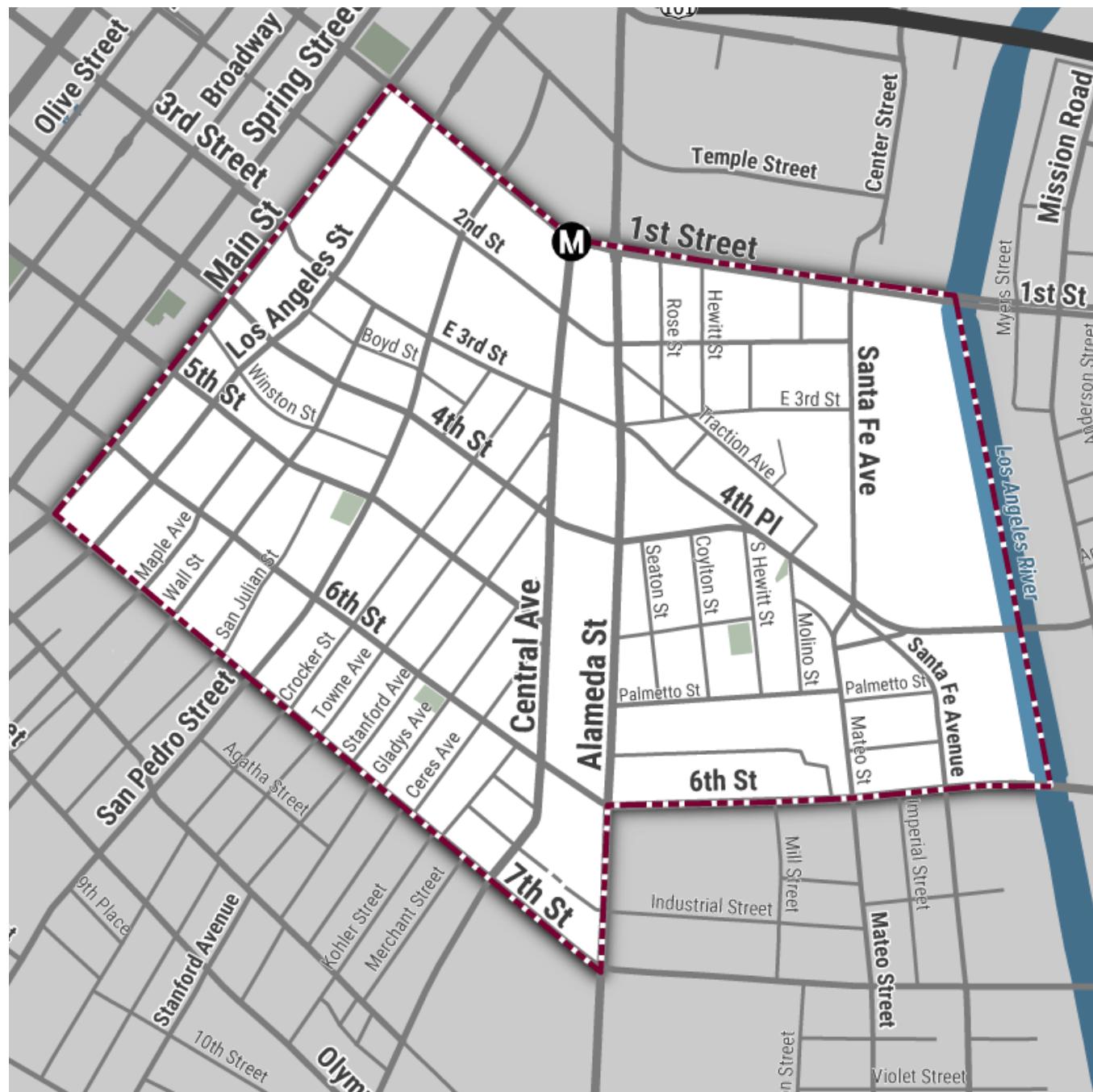
Downtown Neighborhood Profile

Project Area

The Downtown project area as defined by the SRFS team includes 1st Street to the north, Main Street to the west, 7th Street/6th Street to the south, and the Los Angeles River to the east.

(see Map 2). These boundaries were defined by Los Angeles Countywide Statistical Areas (CSAs) and slightly modified by the project team to best address neighborhood needs.

MAP 2 Downtown Neighborhood Project Area



Neighborhood History & Current Conditions

The Downtown project area consists of three neighborhoods: Skid Row, Little Tokyo, and the Arts District. See Appendix C for a neighborhood land use map.

Skid Row

Skid Row in Los Angeles emerged in the late 19th century as a hub for transient railroad workers, with single-room occupancy (SRO) hotels, taverns, and missions built to serve them. The term “Skid Row” originated in Seattle, WA, where “skid roads” were the places that loggers used to move timber to ports. By the 1930s, it had come to describe areas in cities where marginalized communities (often unhoused and low-income) lived and accessed affordable services like bars, boarding houses, and shelters. These neighborhoods reflected broader patterns of economic exclusion and displacement.

Over time, Skid Row became a refuge for the city’s working poor, unemployed, and disabled. By the mid-20th century, many SROs were demolished, cutting affordable housing in half and displacing thousands. In the 1970s, a city commission proposed concentrating housing and services in Skid Row to protect it from gentrification and better support unhoused residents. Since then, the neighborhood has seen improvements in services, nonprofit housing, and supportive infrastructure, though it remains a critical site of poverty and homelessness. The 2024 Greater Los Angeles Homeless Count found that there were 3,791 people experiencing homelessness in Skid Row, with 2,112 of them unsheltered. This is the densest concentration of people experiencing homelessness in Los Angeles County.

Little Tokyo

After the 1882 Chinese Exclusion Act, Japanese immigrants began settling in what became Little Tokyo, establishing a vibrant community of businesses, schools, and cultural institutions. By the early 1940s, over 35,000 Japanese Americans lived and worked near today’s Little Tokyo Historic District.

In 1942, Executive Order 9066 forced the removal and incarceration of over 120,000 Japanese Americans, leaving Little Tokyo nearly empty. After the war, some residents returned to rebuild, but many never came back due to lost homes and widespread urban redevelopment.

Despite these challenges, the neighborhood saw a revival in the 1970s through community-led efforts and the founding of the Little Tokyo Service Center. Today, it is one of only four remaining Japantowns in the U.S., home to a strong *Nisei* (second generation Japanese American) community and rich cultural heritage.

Arts District

After Spanish settlement in the late 18th century, the area now known as the Arts District was primarily agricultural. By the early 20th century, railroads and warehouses emerged to support the growing citrus industry.

Following World War II, industry moved out, leaving behind vacant buildings. In the 1970s, artists began repurposing these spaces as affordable live-work studios, sparking a grassroots arts movement. The neighborhood’s creative energy grew through the 1980s, and by the mid-1990s, the City officially designated it the Arts District. Today, the Arts District remains the home of many artists as well as those in other creative industries, including green technology, architecture, and entertainment, while still retaining some of its industrial use.

Older Adults in Downtown

Skid Row

The older adult population in Skid Row reflects broader national trends in poverty: increasingly women, people of color, and immigrants. A 2014 report by the Downtown Women's Action Coalition found that half of the women surveyed in Skid Row were over 50, and 88 percent were people of color, with most identifying as African American. A 2024 study by the RAND Corporation found that the median age in Skid Row was almost 50. Many older adults on Skid Row have faced lifelong hardships, including mental illness, substance use, and histories of abuse.

Little Tokyo

A PBS SoCal report found that 25 percent of Little Tokyo's population is 65 or older, more than twice the county average. Limited mobility is another concern for older adults in Little Tokyo, as over half of households lack access to a vehicle. Although Little Tokyo has a large population of older people at high risk for social isolation, local infrastructure provides access to other services that decrease the vulnerability of older residents. The neighborhood includes 14 healthcare providers and five financial institutions, helping meet the key needs of older adults. The Little Tokyo Service Center plays a central role, providing resources and culturally relevant support to older residents, including many monolingual Japanese speakers.

Arts District

Compared to the other two neighborhoods in the project area, the Arts District's older adult population is relatively low. According to 2020 U.S. Census data, only 4.7 percent of the population is over the age of 65.

City of Los Angeles	Downtown
Median household income:	
\$69,778	\$37,309
Residents aged 65 and older:	
13%	15%
Renter households:	
63%	90%
Black population:	
8%	16%
Asian population:	
12%	31%
Hispanic or Latino population:	
48%	19%
Residents proficient in English:	
75%	96%

Source: U.S. Census Data, 2020

Transportation

Skid Row and Little Tokyo are well-served by transit options, and have an interconnected street grid that generally supports walkability. Some arterial roadways present barriers, however, and dedicated bicycle facilities are limited to areas east and south of Little Tokyo.

Transit

Downtown is well-served by a number of transit services. Current Metro bus service includes the 16, 18, 20, 33, 40, 51, 53, 55, 60, 62, 92, 460, and 720 routes, providing local and regional connections throughout the Los Angeles region. DASH bus service includes the A route serving Little Tokyo and the Arts District, the E route serving Westlake and the Fashion District, and the D route serving Union Station and South Park. On the northern edge of the neighborhood, the

Metro Little Tokyo/Arts District Station provides rail service for the A and E lines.

Paratransit service in the neighborhood is available through Access and CityRide dial-a-ride. Permanent Access pick-up stands are available just north of the project area at LA Union Station and at the Ahmanson Theater and Music Center. See Appendix D for a map of transit stops and destinations.

Bicycle Facilities

Several bicycle facilities create a network within Downtown and connect to adjacent neighborhoods. There are Class IV separated bike lanes along 4th Place, 5th Street, 6th Street, and a portion of Central Avenue. In the Arts District, Class II striped bike lanes are located on Santa Fe Avenue, Mateo Street, and E 3rd Street. In Little Tokyo, 2nd Street is a signed bike route with shared lane markings. The neighborhood also has eight Metro Bike Share stations. See Appendix E for a map of bike facilities and bikeshare stations.

Multimodal Volumes and Speeds

Motor vehicles traveled at average speeds between 12 and 20 miles per hour in Downtown (according to 2019 StreetLight data). The highest average vehicle speeds (20-25 mph) are seen

TABLE 1 Multimodal volumes

Multimodal Volumes	Streets
Highest pedestrian volumes	7th Street, Alameda Street
Highest bike volumes	Alameda Street
Highest motor vehicle volumes	Alameda Street, Central Avenue

in the industrial areas within the Arts District. Table 1 lists streets with the highest volumes of pedestrians, bicyclists, and motor vehicles; see Appendix F for more detail.

Collisions and Injuries

Los Angeles' City's [High Injury Network](#) (HIN) identifies the 6 percent of city streets where 70 percent of severe injuries and fatalities involving people walking occur. In the Downtown project area, the HIN streets, shown in Map 3, are all multi-lane arterials that serve key commercial destinations in the neighborhood.

Between 2016 and 2020, 60 older adults in Downtown were involved in traffic collisions in the neighborhood, including ten crashes that resulted in fatal or severe injuries (KSIs).

60 collisions happened between 2016 and 2020

(involving older adult pedestrians and bicyclists)

17%

resulted in severe injuries or fatalities

60%

occurred during the daytime

(3% occurred at dusk or dawn and 37% at night)

30%

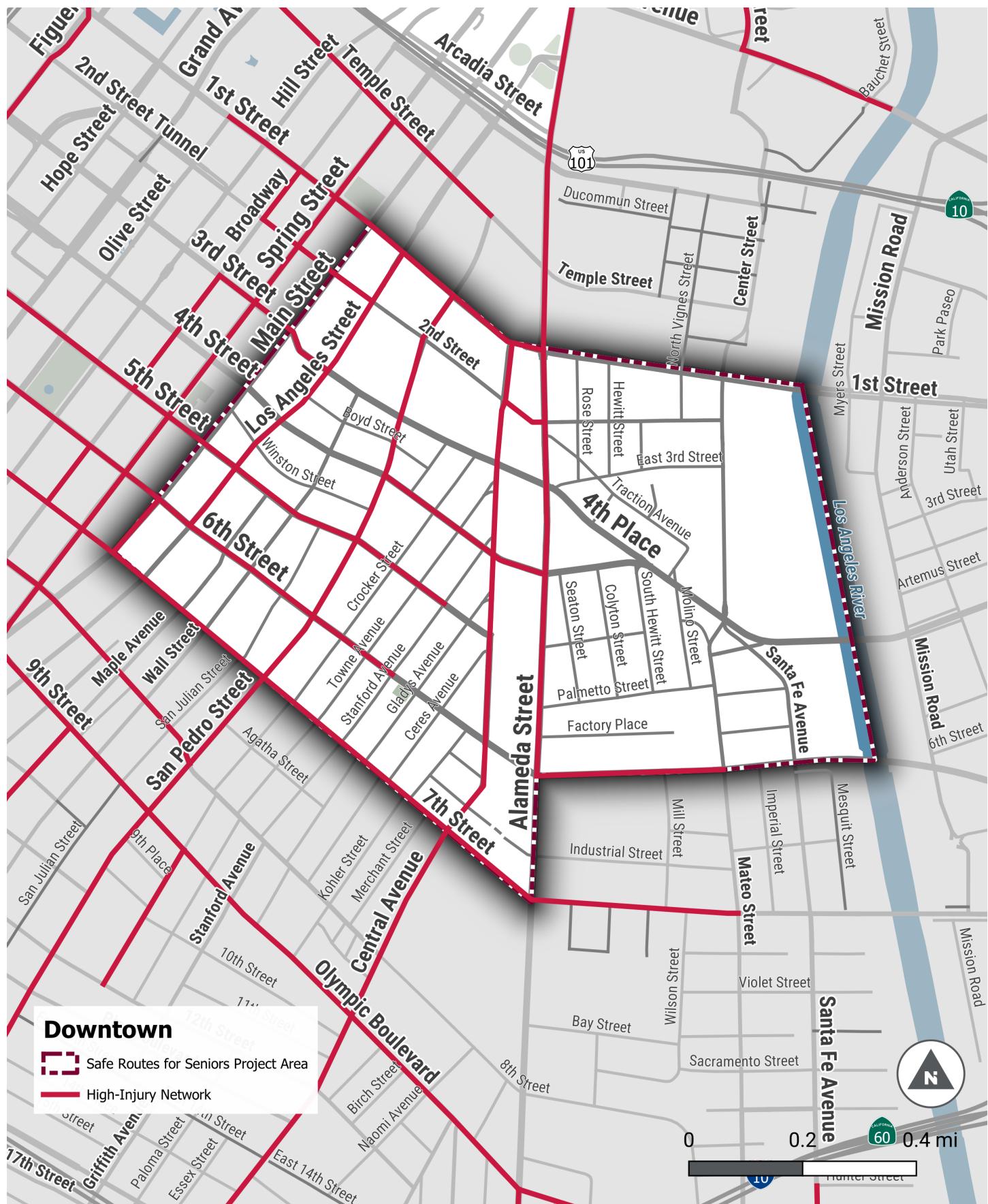
occurred because of violation of pedestrian right-of-way

(33% occurred because of pedestrian violations)

92% occurred at intersections

Source: Transportation Injury Mapping System (TIMS), 2016-2020. See Appendix G for a KSIs map.

MAP 3 High-Injury Streets in Downtown





Chapter 4

Outreach and Engagement

Authentic, meaningful community engagement is a core principle of LADOT's approach to all planning processes. Community members bring insights from their lived experiences and personal knowledge of their neighborhood's built environment and social context. Following review of the demographics, HIN roadways, and community destinations relevant to older adults presented in Chapter 3, LADOT refined the project study area to focus on the Skid Row and Little Tokyo neighborhoods.

In-person outreach was prioritized to address the digital divide and accessibility challenges, though online options for feedback were also created. During the six-month planning process, LADOT convened a **Community-Based Organization (CBO) Advisory Committee**, a coalition of service providers that work directly with older adults in Downtown to steer outreach and help the project team connect with hard-to-reach older adults (including non-English speakers and low-income or unhoused older adults). Working with this committee, LADOT engaged face-to-face with almost 300 older adults across Downtown, with special focus on Skid Row and Little Tokyo.

Residents had multiple avenues to share where and how they travel through the neighborhood, from pop-up events at Blue Hollywood Street Sanctuary to intercept surveys at Japanese Village Plaza. See Appendix H for the full SRFS Outreach and Engagement Strategy.

Outreach, Promotion, & Incentives

The project team conducted outreach through:

- **Project Website** regularly updated with event information and a link to the survey.
- **Senior Housing** visit to residents at Little Tokyo Towers
- **Pop-ups at events** hosted by members of the CBO Advisory Committee, including the Little Tokyo Service Center Terasaki Budokan weekly ping pong class for seniors, The People Concern's weekly open studio session and artists meeting, and the Blue Hollywood Street Sanctuary "Coffee and Cuts" program.
- **Intercept Surveys** at the Festival for All Skid Row Artists in Gladys Park, the Source Resource Fair at the Los Angeles Central Library, Japanese Village Plaza, Little Tokyo/Arts District A/E Line Station, Arts District Park, and local bus stops.
- **Incentives** like gift cards to grocery stores and restaurants were provided to participants at pop-ups and intercept surveys as a small way to compensate community members for sharing their valuable lived experience with the project team.



The Community-Based Organization Advisory Committee

Ongoing engagement with representatives from community-based organizations (CBO) who serve older adults in Downtown provided insight into daily challenges, needs, and priorities that the SRFS project should address. CBO Advisory Committee members represented a wide spectrum of organizations in Little Tokyo and Skid Row, ensuring that diverse perspectives from agencies serving unhoused and low-income seniors were highlighted. Organizations represented included faith-based shelters and supportive-housing providers, an arts-driven theater collective, a social-service and community-development agency, a workforce-development nonprofit, and a grassroots harm-reduction street sanctuary. The CBO Advisory Committee met multiple times over the course of the project, shared information about the project with their constituencies, and hosted the project team at multiple ongoing events and programming.

Thank you so much for always caring about the safety of the seniors
- Jane Kim



CBO Advisory Committee Member

Quincy "Pastor Blue" Brown



“ LADOT'S Safe Routes for Seniors program is important to our organization (Blue Hollywood Street Sanctuary) because the majority of our participants are seniors, many are disabled in some capacity and nearly all are getting around on foot — and we have regularly witnessed individuals getting struck by vehicles or otherwise struggling to navigate the downtown city streets. We welcome this project's mission to re-envision street design around our Skid Row community and make navigating the neighborhood safer.

I've been inspired by the enthusiasm the committee showed in attacking the long-ignored transportation concerns in our neighborhood. I've learned that some areas we have dubbed as naturally dangerous are actually poor design, and thus can be modified with consistently safer outcomes.

In general, there are not enough highly visible crosswalks, the crossing time for pedestrians, especially elderly pedestrians, is too short, and there are many broken curbs which can cause accidents. Additionally, traffic moves too fast on many streets, creating dangerous conditions, and could be improved by speed bumps. The recommendations of this program would rectify many of those issues. **”**

Community Engagement Activities

For a full detailed list of engagement activities, refer to Appendix I.



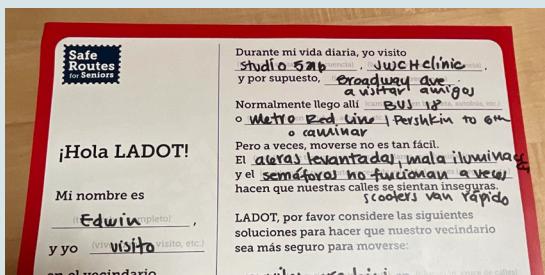
October – December, 2024:

Intercept Surveys: 35 older adults shared their key destinations and locations where they experience transportation safety issues.

Senior Housing Visit: Little Tokyo Towers residents discussed the places they travel to most and the areas where they feel unsafe getting there.

CBO Advisory Committee Meeting #1:

Members shared opportunities to engage with hard-to-reach older adults in Downtown and mapped out popular destinations and transportation issues.



February 2025:

Pop ups: 131 older adults participated in a interactive Mad Libs activity to share their frequent destinations, transportation challenges, and improvements they'd like to see.

March 2025:

CBO Advisory Committee Meeting #2: Members provided feedback on proposed focus areas for improvements based on community engagement and shared opportunities for further public outreach.



May – June 2025:

CBO Advisory Committee Meeting #3: Members offered input on draft recommendations.

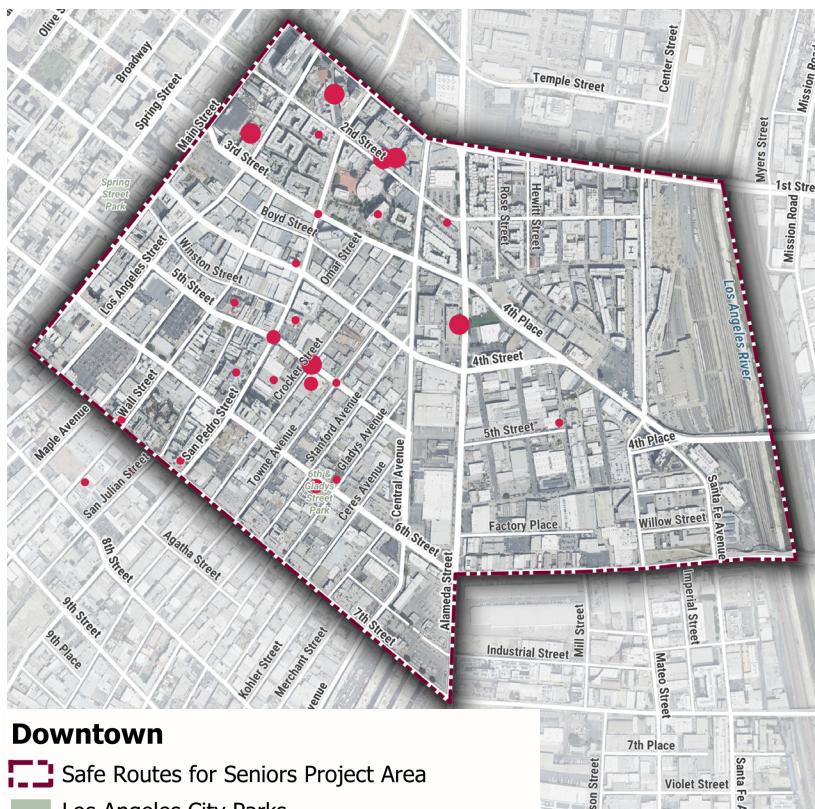
Pop-ups: 104 older adults provided feedback on proposed transportation improvements.



Chapter 5

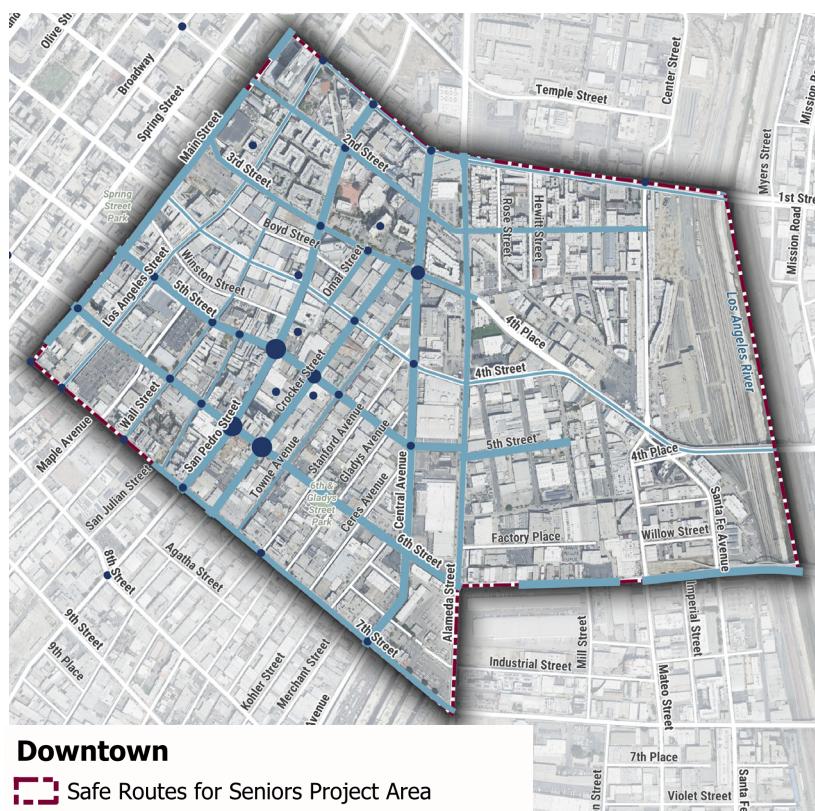
Neighborhood Mobility Opportunities and Challenges

MAP 4 Community-identified issues and destinations



Downtown

- Safe Routes for Seniors Project Area
- Los Angeles City Parks
- Community member-identified key destinations



Downtown

- Safe Routes for Seniors Project Area
- Los Angeles City Parks
- Community member-identified issue locations
- Corridors with high number of issue locations

Key Destinations, Issue Locations, and Modes Used

Destinations and Issue Locations:

To help understand mobility opportunities and challenges in Downtown, older adults were asked to share locations they frequent as well as areas where they experience transportation safety issues. Popular destinations were mainly located in Skid Row and Little Tokyo, and included shopping areas like Japanese Village Plaza and Little Tokyo Marketplace as well as service providers like Union Rescue Mission, Los Angeles Mission, the Ron Beasley Wellness Center, and the Downtown Womens Center. The Los Angeles Central Library, The Bin, and Terasaki Budokan were also frequented often by older adults.

Transportation issues were clustered on Crocker Street, San Pedro Street, and Central Avenue. Older adults in Little Tokyo stated that driver speeding was a significant issue along with narrow sidewalks. In Skid Row, older adults discussed the need for better lighting at night and issues crossing streets safely.

Transportation Modes: Responses from project surveys indicated that older adults in Downtown primarily walk or use a mobility device to get around (see Figure 1). Surveys also revealed that many older adults in Downtown experience ambulatory difficulties related to walking and balance (see Figure 2).

FIGURE 1 How do you usually get around in Downtown?

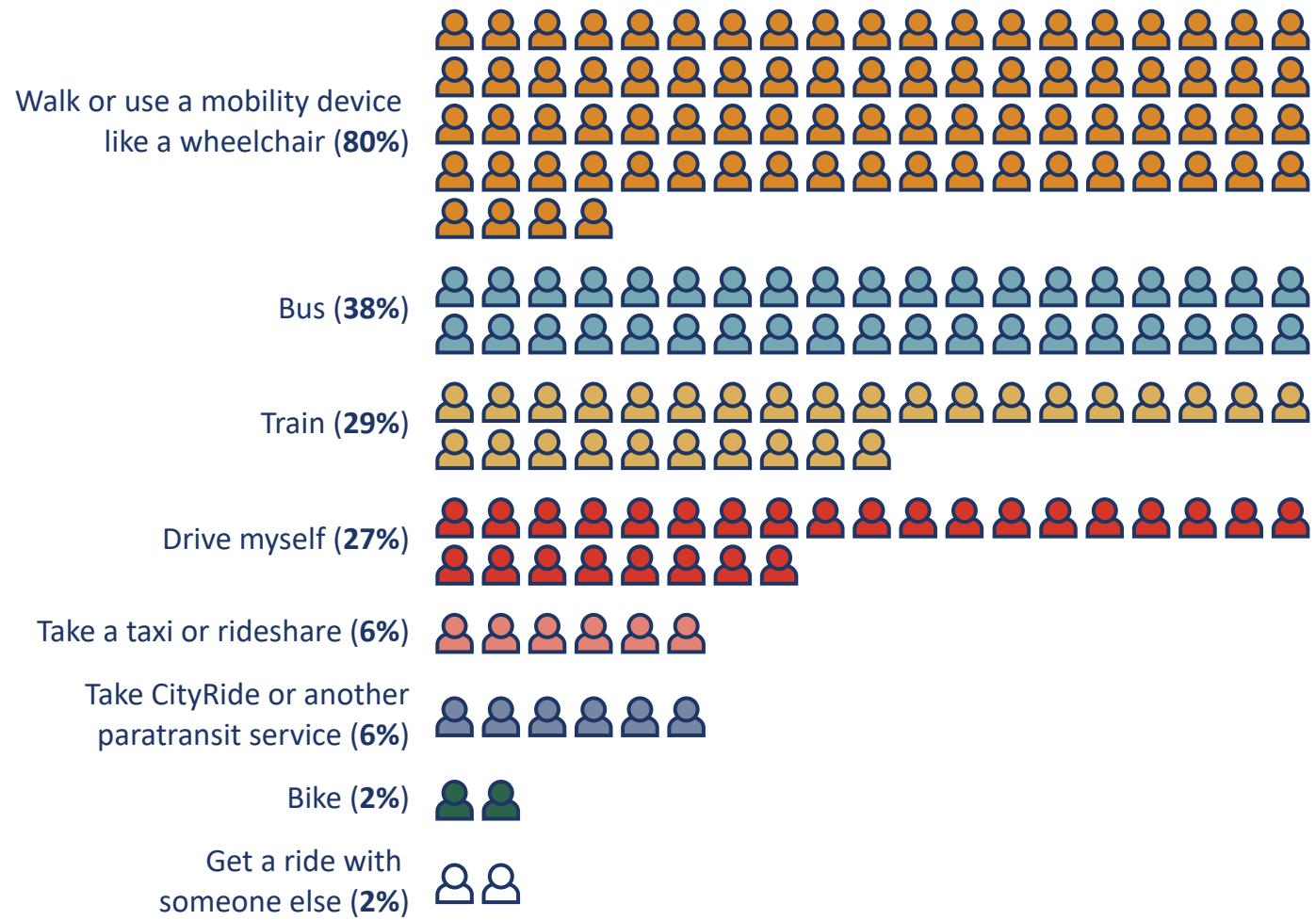
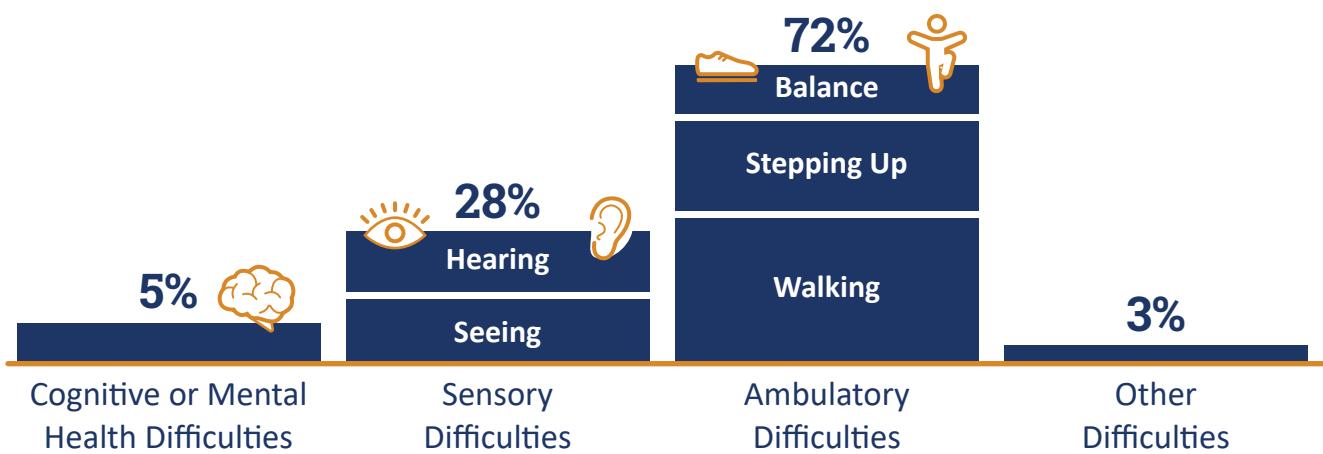


FIGURE 2 What difficulties do you experience that affect your daily life?



Mobility Opportunities and Challenges

The project team combined insights from community engagement activities, existing conditions analysis, and data from neighborhood field visits to identify mobility opportunities and challenges for older adults in Downtown.

Poor sidewalk conditions

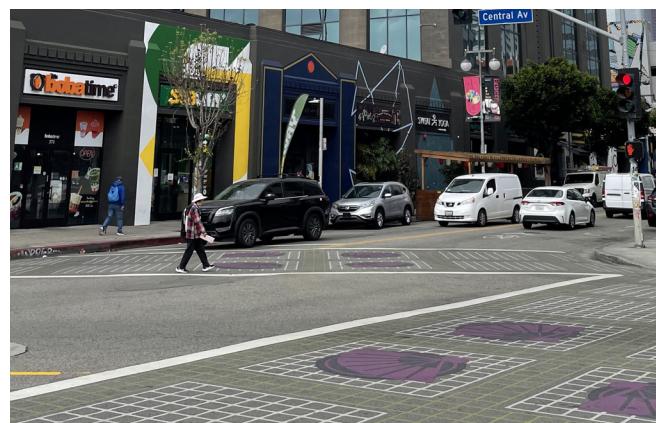
Older adults noted that sidewalks along 2nd Street and 3rd Street in Little Tokyo are uneven and poorly maintained, creating tripping hazards and making navigation difficult, especially for older adults and people using mobility devices. 33 percent of survey respondents stated that their top concern traveling in the neighborhood was the poor condition or absence of sidewalks.



Uneven sidewalk in Little Tokyo

Crossing conflicts at intersections

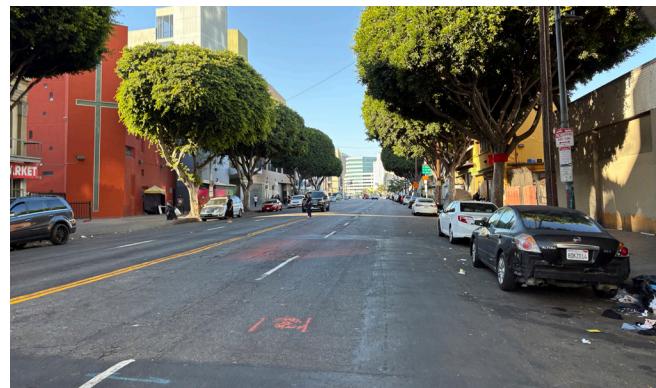
Crossing the street safely is a concern in both Little Tokyo and Skid Row. In Little Tokyo, community members highlighted the busy mid-block crossing on 2nd Street used to access Japanese Village Plaza, noting that the signal doesn't provide enough time for older adults to cross safely. In Skid Row, many crossings are either uncontrolled or missing crosswalks.



Pedestrian crossing the street at the intersection of Central Avenue and 2nd Street.

Long distances between safe crossings

In Skid Row, particularly along San Pedro Street, long blocks make it difficult for pedestrians to find safe and convenient places to cross. This creates barriers to mobility and increases exposure to traffic.



Long segment of San Pedro Street without marked crossings.

Night-time visibility and lighting

Poor lighting in Skid Row contributes to low visibility at night, making it harder for pedestrians to feel safe and be seen by drivers, especially in areas with high foot traffic.

Beautification and greening

Many community members in Skid Row noted that the streets are not clear, and the neighborhood streetscape remains largely industrial and unwelcoming. Older adults emphasized the need for more trees and greenery to create a more pleasant environment for walking and biking.

Driver speeding

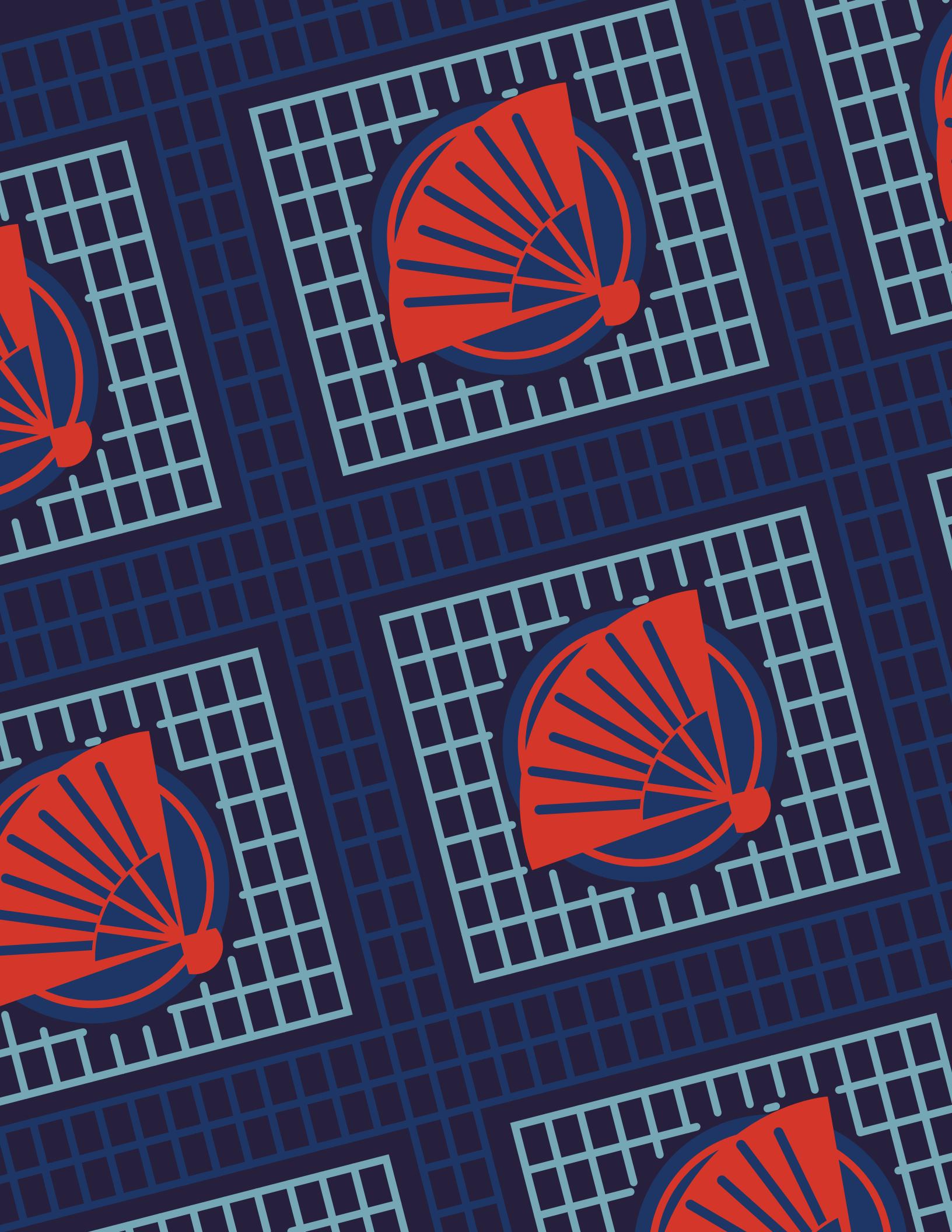
High vehicle speeds are a widespread concern, especially throughout Skid Row and along 2nd Street in Little Tokyo – 44 percent of survey respondents stated that their top concern about traveling in Downtown was that people drive too fast. These conditions discourage walking and increase risk for vulnerable road users.



Lack of street trees or landscaping on 4th Street.



San Pedro Street





Chapter 6

Recommendations

Recommendations

The infrastructure recommendations in this plan aim to maximize positive impacts on the mobility, safety, and health of older adults. Research shows that multimodal infrastructure investments are associated with increases in walking and biking trips across age groups, including older adults.⁴ These improvements not only support active transportation, but also contribute to physical and mental well-being by encouraging regular activity and reducing isolation among older populations.

Based on community feedback and analysis of existing conditions, the project team developed recommendations to improve safety along multiple streets in Skid Row and Little Tokyo.

While many of the recommended improvements could be made at additional locations throughout the neighborhood, the corridors selected in this plan reflect the following priorities:

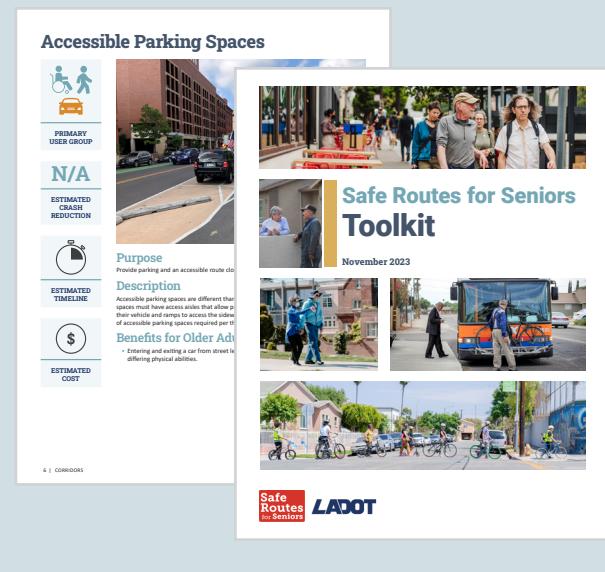
- Locations where analysis and outreach identified transportation safety issues
- Popular destinations for older adults who live, work, or frequent the project area

Project prioritization typically involves an assessment of key factors such as safety, demand, connectivity, and equity. In the SRFS project, those factors were considerations in both selecting the study area and the planning process; hence all included recommendations reflect those factors. The following pages map out the recommendations (see Map 5 and Map 6) and include a detailed table of all recommendations across the project area.

Safe Routes for Seniors Toolkit

Recommendations draw from infrastructure treatments in the [Safe Routes for Seniors Toolkit](#), which was developed to illustrate elements that improve safety, mobility, and accessibility for older adults who walk, bike, and roll.

The toolkit is organized into five topic areas: Corridors, Crossings and Intersections, Transit, Bicycle Facilities, and Street Elements (example pages included here). The estimated crash reduction, cost, and timeline is included for each treatment. Drawing on best practices from city, state, and national resources, the toolkit was used to develop recommendations in the Plans and is intended to serve as an ongoing resource for communities and LADOT planning and engineering teams.



The image shows a screenshot of the Safe Routes for Seniors Toolkit page for Accessible Parking Spaces. The page is divided into two main sections: a sidebar on the left and a main content area on the right.

Accessible Parking Spaces

PRIMARY USER GROUP: N/A

ESTIMATED CRASH REDUCTION: N/A

ESTIMATED TIMELINE: N/A

ESTIMATED COST: N/A

Purpose: Provides parking and an accessible route for people with disabilities.

Description: Accessible parking spaces are different than regular parking spaces as they must have access routes that allow people with disabilities to get to the space. The number of accessible parking spaces required per lot varies by location.

Benefits for Older Adults:

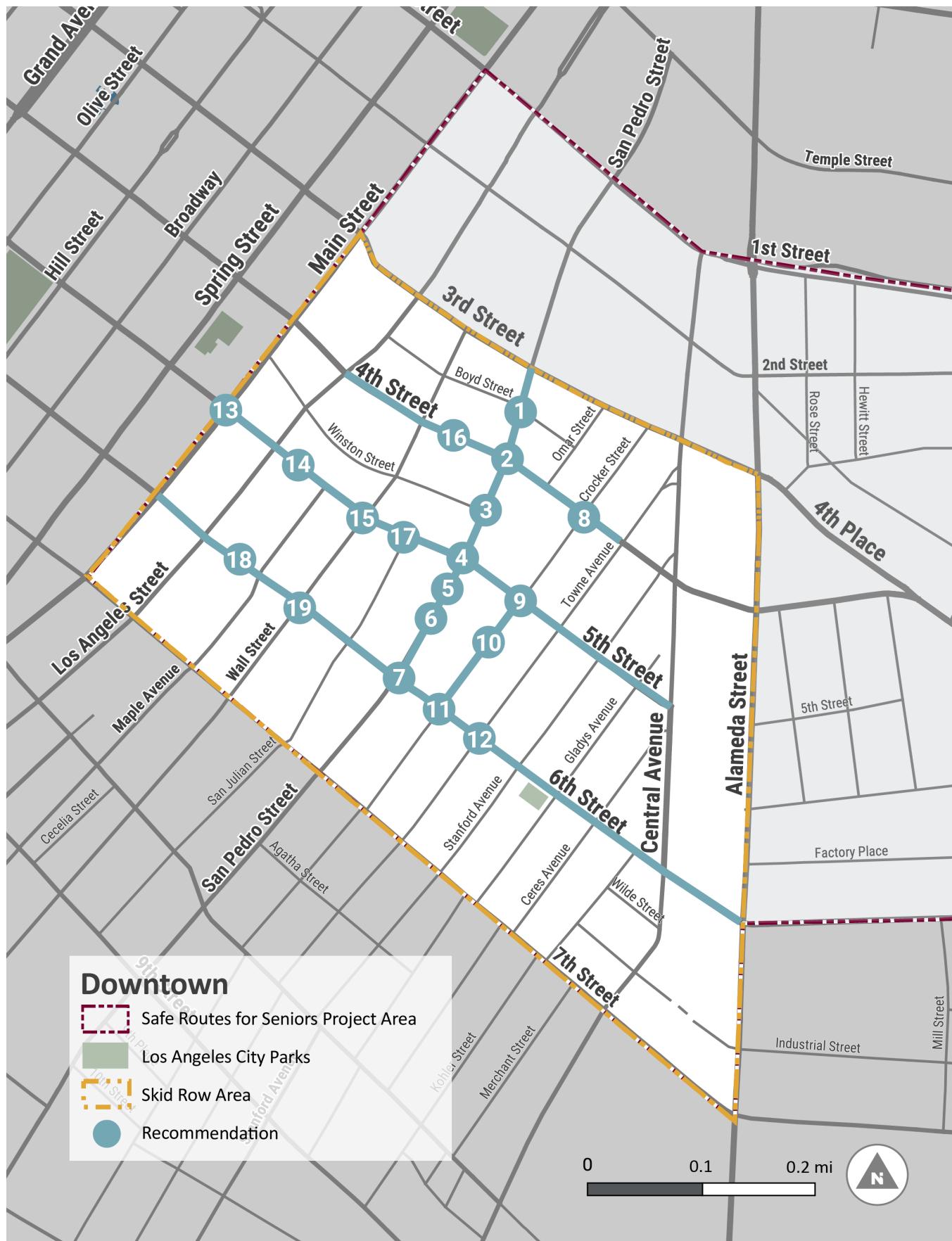
- Entering and exiting a car or street is easier, offering physical abilities.

Safe Routes for Seniors Toolkit
November 2023

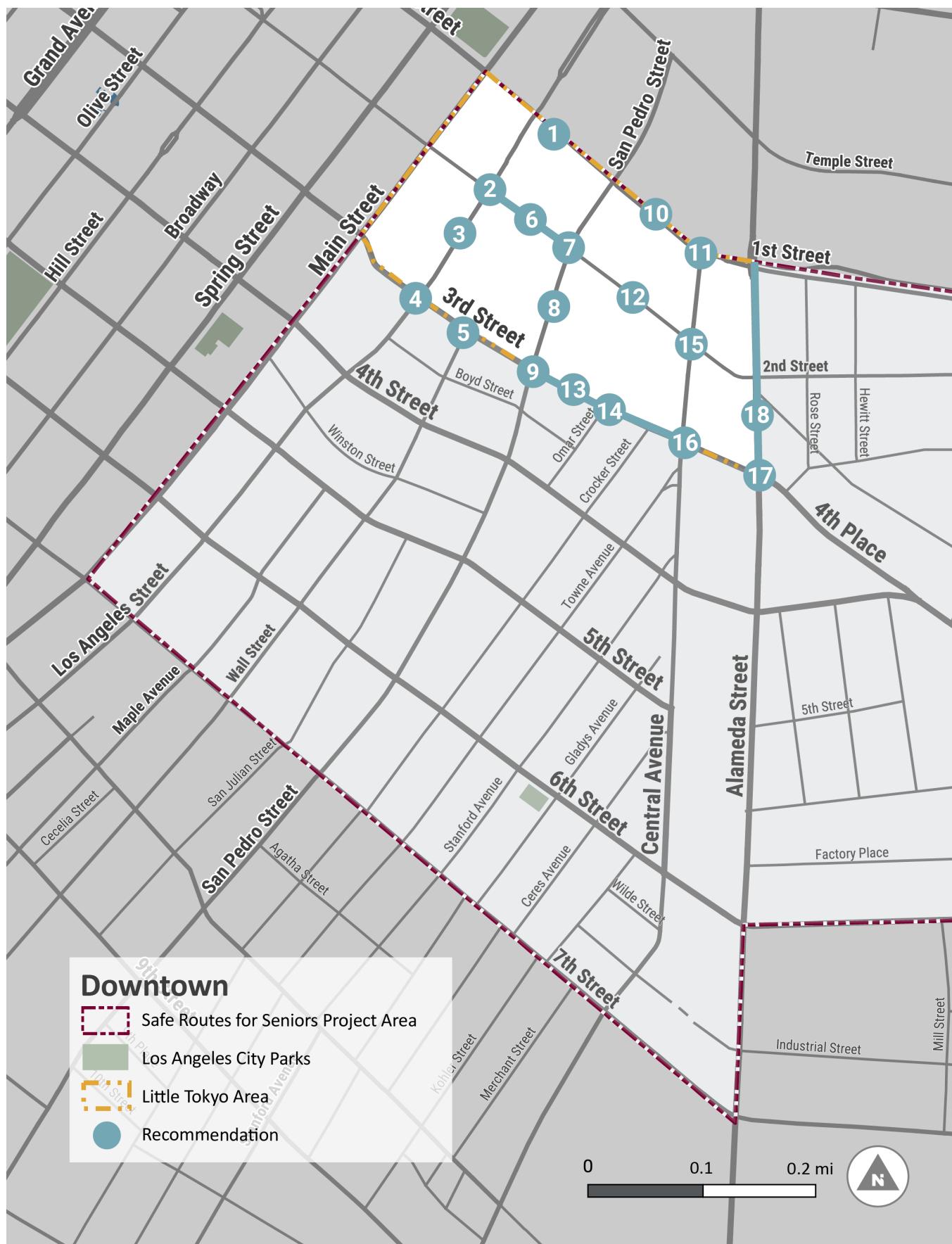
LADOT

⁴ Stoker, P., Ewing, R., Wineman, J., & Handy, S. (2015). Proactive planning for healthy communities: Integrating age-friendly community planning and active transportation. *Journal of Aging and Health*.

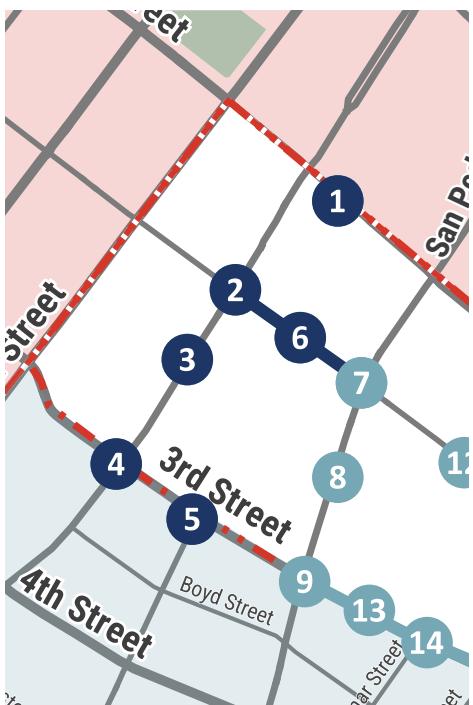
MAP 5 Downtown Recommendations: Skid Row Area



MAP 6 Downtown Recommendations: Little Tokyo Area



West Little Tokyo



1 1st Street and Astronaut Ellison S Onizuka Street

- Add high-visibility crosswalk across Onizuka Street



2 2nd Street and Los Angeles Street

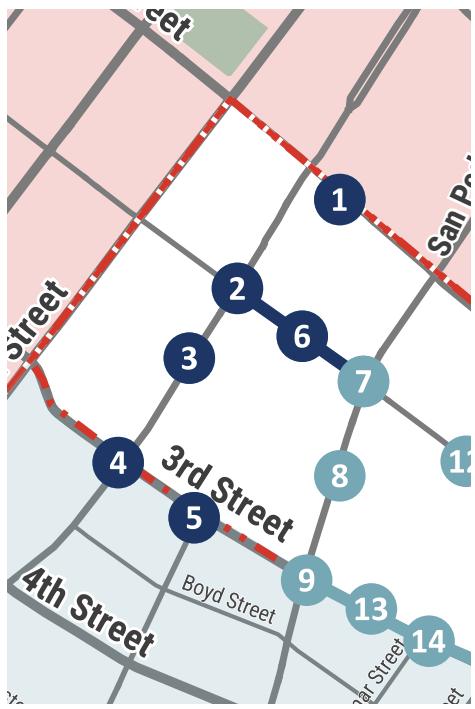
- Increase pedestrian crossing time across Los Angeles Street
- Install protected left turn signals



3 Los Angeles Street and Toyo Miyatake Way

- Add curb extension on east side of existing crosswalk





4

Los Angeles Street and 3rd Street

- Convert floating curb extension and wedge to permanent extensions
- Increase pedestrian crossing time across Los Angeles Street



5

Wall St and 3rd Street

- Add Rapid Rectangular Flashing Beacon (RRFB) to existing crossing
- Add curb ramp and curb extension to north side of crossing; upgrade curb ramp on south side of crossing if feasible
- Upgrade existing painted buffer zone and plastic posts with permanent concrete separation



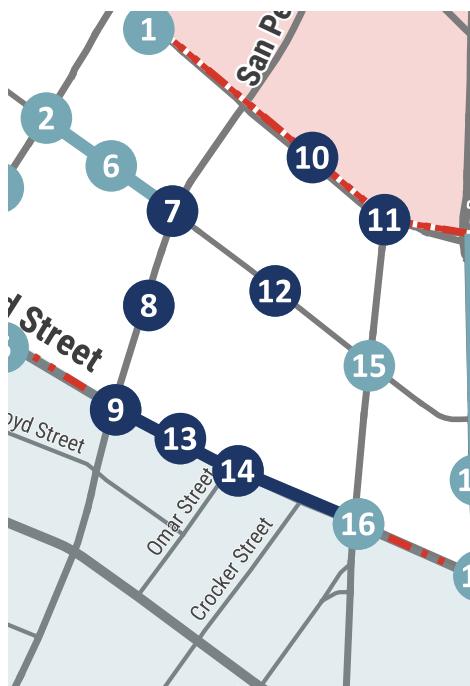
6

2nd Street, Los Angeles Street to San Pedro Street

- Reconstruct or repair sidewalk where uneven



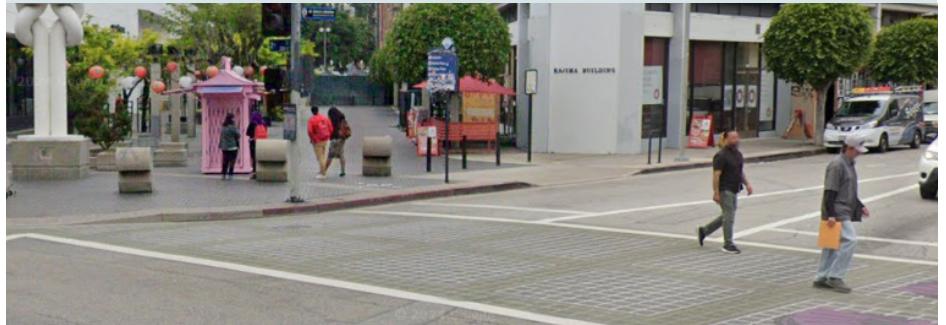
Central Little Tokyo



7

2nd Street and San Pedro Street

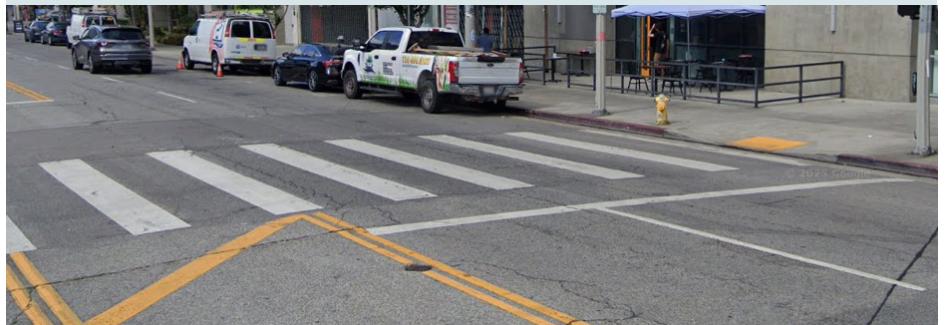
- Install curb extensions on all intersection corners
- Upgrade curb ramps to perpendicular
- Increase pedestrian crossing time across San Pedro Street



8

San Pedro Street and Toyo Miyatake Way

- Add pedestrian refuge island
- Add curb extensions on both sides of existing mid-block crosswalk
- Increase pedestrian crossing time

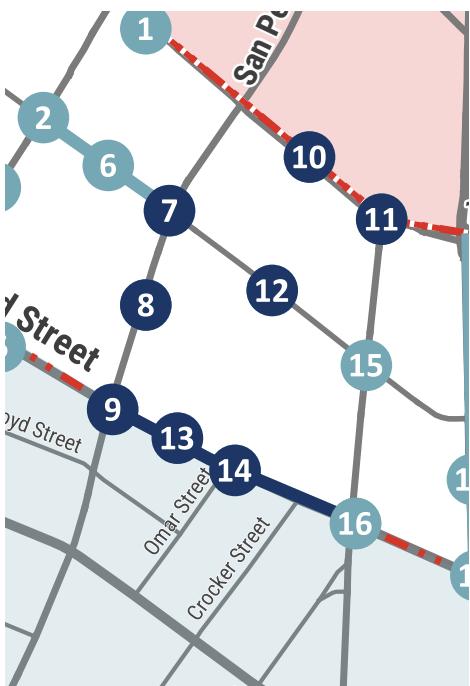


9

San Pedro Street and 3rd Street

- Add northbound protected left turn phase to signal
- Extend pedestrian crossing time across San Pedro Street
- Upgrade curb ramps, and convert floating curb extension to permanent





10

1st Street, midblock, between San Pedro Street and Central Ave

- Add curb extensions to both sides of existing mid-block crosswalk



11

1st Street and Central Avenue

- Add new curb extension on southeast corner of intersection
- Increase pedestrian crossing time across 1st Street and study the feasibility of a pedestrian scramble

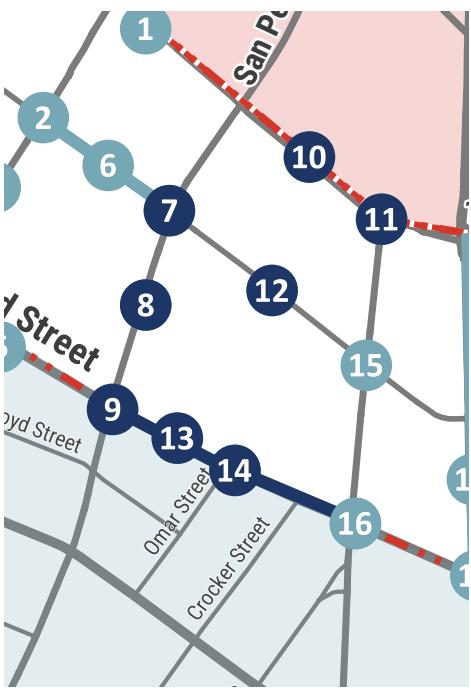


12

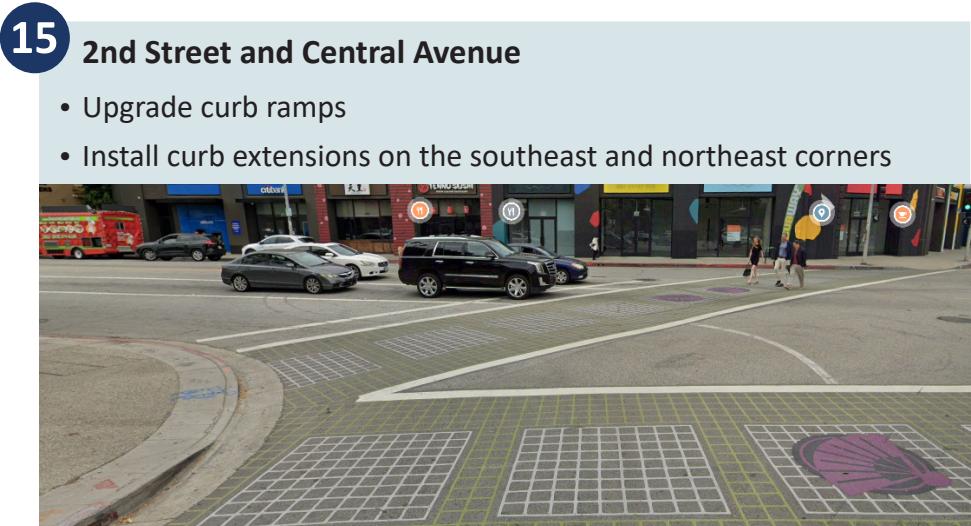
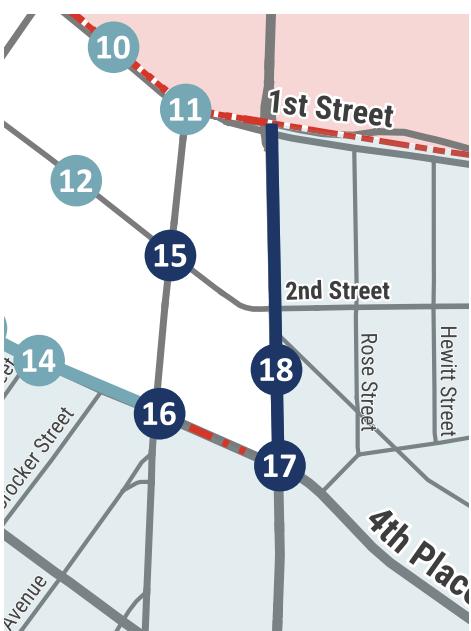
2nd Street, midblock between Central Avenue and San Pedro St

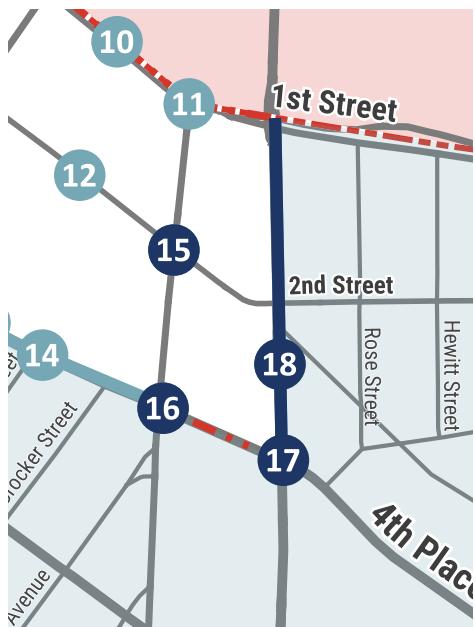
- Add new curb extensions and upgrade curb ramps at existing mid-block crosswalk
- Shorten waiting time for crossing pedestrians





East Little Tokyo





16

Central Avenue and 3rd Street

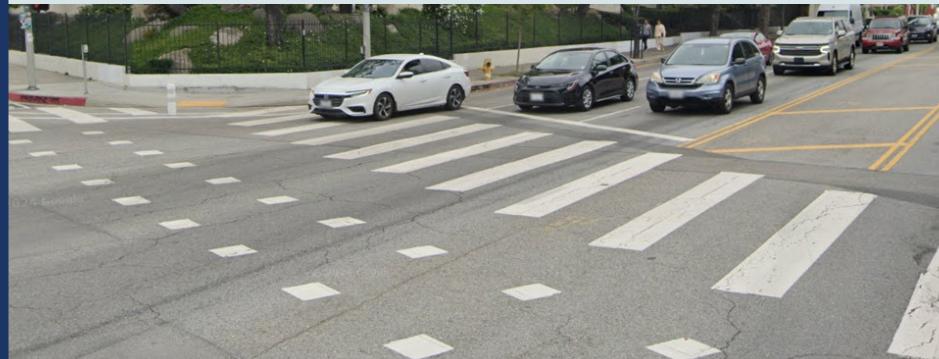
- Add curb extensions at the northwest and northeast corners of the intersection; upgrade the painted bike lane buffer into raised curb extension



17

Alameda Street and 3rd Street

- Increase pedestrian crossing time across 3rd Street
- Add pedestrian refuge island
- Add protected only left turn signal to northbound Alameda Street
- Remove southbound right-turn lane and convert to curb extension



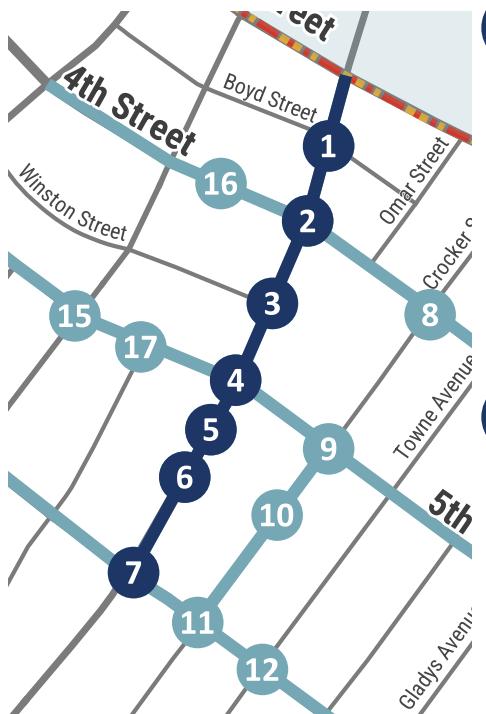
18

Alameda Street, 1st Street to 3rd Street

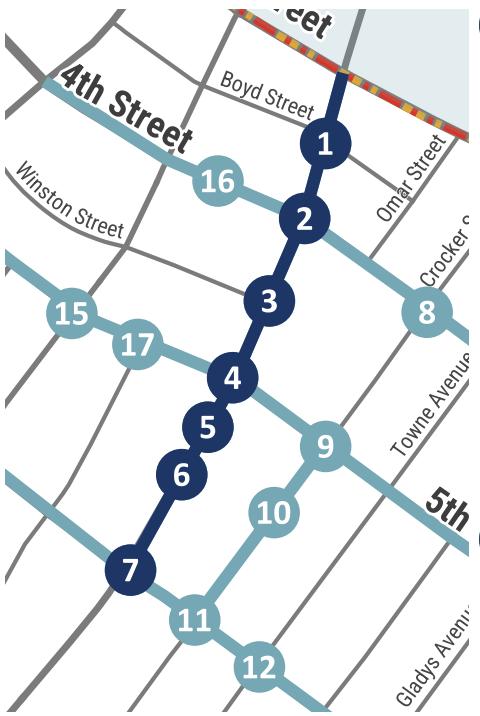
- Add infill pedestrian-scale lighting along corridor



Central Skid Row



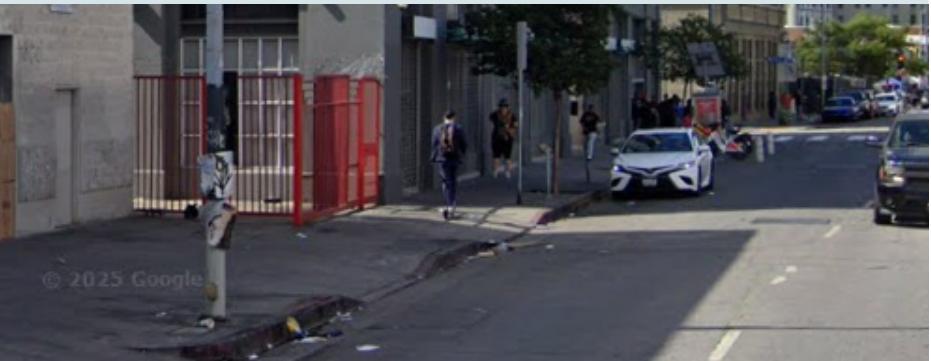
- 1 San Pedro Street, 3rd Street to 5th Street**
 - Add high visibility crosswalks along minor street and alley crossings
- 2 4th Street and San Pedro Street**
 - Add curb extension to northeast corner of the intersection
- 3 San Pedro Street and Winston Street**
 - Add curb extension to northeast corner of the intersection
 - Upgrade uncontrolled crosswalk with a Pedestrian Hybrid Beacon
- 4 San Pedro Street and 5th Street**
 - Install transit shelter
 - Add curb extensions to the southeast and northeast corners of the intersection
 - Review intersection for increased pedestrian crossing time across San Pedro Street



5

San Pedro Street, 4th Street to 6th Street

- Add pedestrian-scale lighting along corridor



6

San Pedro Street, between 5th and 6th Street

- Install mid-block crossing with a Pedestrian Hybrid Beacon



7

San Pedro Street and 6th Street

- Study intersection for a pedestrian scramble phase
- Install curb extension on northwest corner
- Increase pedestrian crossing time across San Pedro Street



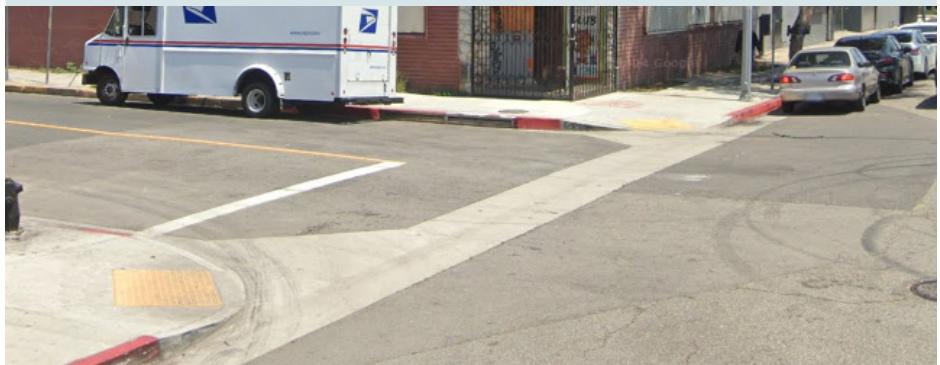
East Skid Row



8

Crocker Street and 4th Street

- Add high-visibility crosswalks across Crocker Street and Omar Street



9

Crocker Street and 5th Street

- Install Rectangular Rapid Flashing Beacon (RRFB) and high-visibility crosswalk across 5th Street



10

Crocker Street, 5th Street to 6th Street

- Add pedestrian-scale lighting along block





11

Crocker Street and 6th Street

- Install Rectangular Rapid Flashing Beacon (RRFB) and high-visibility crosswalk across 6th Street



12

Towne Avenue and 6th Street

- Install transit shelter on southwest corner of intersection



West Skid Row



13 Main Street and 5th Street

- Install transit shelter



14 5th Street, Main Street to Central Avenue

- Upgrade bike lane with hardened physical separation materials



15 5th Street and Wall Street

- Add curb extension to southwest corner of intersection



16 4th Street, Los Angeles Street to Towne Avenue

- Add shade trees along corridor





17

5th Street and San Julian Street

- Convert existing crossing to a Pedestrian Hybrid Beacon
- Expand sidewalk south of 5th Street



18

6th Street, Main Street to Alameda Street

- Upgrade bike lane with hardened physical separation materials



19

6th Street and Wall Street

- Extend pedestrian crossing time across 6th Street



Detailed Recommendations List

Table 2 and Table 3 include details about each location's specific issues and proposed recommendations. To support future implementation, both tables also provide planning-level cost opinions, a rating of implementation complexity, and includes whether or not external funding through grants or other sources and partnerships outside of LADOT is required for implementation. See Appendix J for information on maintenance responsibilities for the recommended improvements.

The cost opinions included in Table 2 and Table 3 represent high-level estimations based

on the type and quantity of recommended improvements, with contingencies included to reflect additional costs such as design and mobilization. Costs will be further refined as projects are developed. Opinions are grouped into three categories corresponding with the following ranges: low (lower than \$50,000), medium (\$50,000 - \$200,000) and high (more than \$200,000).

LADOT will leverage ongoing/future projects or apply for grant funding for implementation of recommendations with medium or long-term complexity.

TABLE 2 Little Tokyo Recommendations

Location		Category	Recommendation	Cost Opinion*	Implementation Complexity (Short/Medium/Long Term)	External funding / coordination required
Issue						
1st Street and Astronaut Ellison S Onizuka Street						
Concern about drivers not yielding to pedestrians at crossings	Crossing Enhancement	Add high-visibility crosswalk across Onizuka Street	Low	Short	No	
2nd Street and Los Angeles Street						
Long crossing distance	Signal modification	Increase pedestrian crossing time across Los Angeles Street	Low	Short	No	
Concern about drivers not yielding to pedestrians at crossings	Signal modification	Install protected left turn signals on all approaches	High	Short	No	
Los Angeles Street and Toyo Miyatake Way						
Long crossing distance	Curb ramp/extension	Add curb extension to east side of existing crosswalk	Medium	Medium	Yes	

Location					
Issue	Category	Recommendation	Cost Opinion*	Implementation Complexity (Short/Medium/Long Term)	External funding / coordination required
Los Angeles Street and 3rd Street					
Long crossing distance; Concern about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Convert floating bollard curb extension and wedge to permanent extensions	Medium		Yes
Long crossing distance	Signal modification	Increase pedestrian crossing time across Los Angeles Street	Low	Short	No
Wall Street and 3rd Street					
Concern about drivers not yielding to pedestrians at crossings	Crossing enhancement	Add Rapid Rectangular Flashing Beacon (RRFB) to existing crossing	Medium	Medium	No
Curb ramp not aligned to crossing	Curb ramp/extension	Add curb ramp and curb extension to north side of crossing; upgrade curb ramp on south side of crossing if feasible	Medium	Medium	Yes
Long crossing distance; Concern about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Upgrade existing painted buffer zone and plastic posts with permanent concrete separation	Medium	Long	Yes
2nd Street, Los Angeles Street to San Pedro Street					
Uneven sidewalk poses walking hazard for older adults	Sidewalk	Reconstruct or repair sidewalk where uneven due to driveways on north side of 2nd Street	Medium	Long	Yes
2nd Street and San Pedro Street					
Long crossing distance; Concern about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Install curb extensions on all intersection corners	High	Long	Yes
Curb ramps not aligned to crosswalks	Curb ramp/extension	Upgrade curb ramps to perpendicular	High (with curb extensions recs.)	Medium	Yes

Location					
Issue	Category	Recommendation	Cost Opinion*	Implementation Complexity (Short/Medium/Long Term)	External funding / coordination required
Inadequate time to cross the intersection, presenting barrier to older adults	Signal modification	Increase crossing times across San Pedro Street	Low	Short	No
Long crossing distance	Crossing enhancement	Remove southbound right turn lane on San Pedro Street to create space for curb extension	High (with curb extensions recs.)	Medium	Yes
San Pedro Street and Toyo Miyatake Way					
Long crossing distance	Crossing enhancement	Add pedestrian refuge island to existing crosswalk	High	Medium	Yes
Long crossing distance	Curb ramp/extension	Add curb extensions to both sides of existing mid-block crosswalk	Medium	Medium	Yes
Inadequate time to cross the intersection, presenting barrier to older adults	Signal modification	Increase pedestrian crossing time	Low	Short	No
San Pedro Street and 3rd Street					
Concern about drivers not yielding to pedestrians at crossings	Signal modification	Add northbound protected left turn phase to signal	High	Short	No
Inadequate time to cross the intersection, presenting barrier to older adults	Signal modification	Increase pedestrian crossing time across San Pedro Street	Low	Short	No
Curb ramps not aligned to crosswalks	Curb ramp/extension	Upgrade curb ramps to perpendicular, as feasible	High	Medium	Yes
Long crossing distance; Concern about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Convert floating bollard curb extension to permanent on southeast corner	High	Long	Yes
1st Street, midblock, between San Pedro Street and Central Ave					
Long crossing distance; Concern about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Add curb extensions to both sides of existing mid-block crosswalk	Medium	Medium	Yes

Location					
Issue	Category	Recommendation	Cost Opinion*	Implementation Complexity (Short/Medium/Long Term)	External funding / coordination required
1st Street and Central Avenue					
Long crossing distance; Concern about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Add new curb extension to southeast corner of intersection	Medium	Medium	Yes
Inadequate crossing time	Signal modification	Increase pedestrian crossing time across 1st Street and study feasibility of a pedestrian scramble phase	Low	Medium	No
2nd Street, midblock, between Central Avenue and San Pedro Street					
Long crossing distance	Curb ramp/extension	Add new curb extensions and upgrade curb ramps at existing mid-block crosswalk	Medium	Medium	Yes
Pedestrian signal requires long wait time	Signal modification	Shorten waiting time for crossing pedestrians	Low	Short	No
3rd Street, San Pedro Street to Central Avenue					
Uneven surfaces impede ADA access	Sidewalk	Repair sidewalk where uneven due to driveways or tree roots	Low	Long	Yes
Omar Street and 3rd Street					
Long crossing distance; Concern about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Install floating curb extension in bollard buffer space on south side of 3rd Street	Medium	Long	Yes
2nd Street and Central Avenue					
Curb ramps not aligned to crosswalks	Curb ramp/extension	Upgrade curb ramps on northwest, southeast, and northeast corners to perpendicular	Medium	Medium	Yes
Long crossing distance; Concern about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Install curb extensions on the southeast and northeast corners	High	Medium	Yes

Location					
Issue	Category	Recommendation	Cost Opinion*	Implementation Complexity (Short/Medium/Long Term)	External funding / coordination required
Central Avenue and 3rd Street					
Long crossing distance; Concern about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Add curb extensions at the northwest and northeast corners of the intersection; upgrade the painted bike lane buffer into raised curb extension	High	Medium	Yes
Alameda Street and 3rd Street					
Inadequate time to cross intersection	Signal modification	Increase pedestrian crossing time across 3rd Street	Low	Short	No
Concern about drivers not yielding to pedestrians at crossings	Crossing enhancement	Add pedestrian refuge island on north crossing	High	Medium	Yes
Concern about drivers not yielding to pedestrians at crossings	Signal modification	Upgrade northbound Alameda Street left turn signal to protected only	Medium	Short	No
Long crossing distance	Curb ramp/extension	Remove southbound right-turn lane and convert to curb extension	High	Medium	Yes
Alameda Street, 1st Street to 3rd Street					
Inadequate lighting along corridor	Lighting	Add infill pedestrian-scale lighting along corridor	High	Long	Yes

*Cost opinions were developed based on sources available at the time of plan completion.

TABLE 3 Skid Row Recommendations

Location					
Issue	Category	Recommendation	Cost Opinion*	Implementation Complexity (Short/Medium/Long Term)	External funding / coordination required
San Pedro Street, 3rd Street to 5th Street					
Concerns about drivers not yielding to pedestrians at crossings	Crossing enhancement	Add high visibility crosswalks along minor street and alley crossings	Low	Short	No
4th Street and San Pedro Street					
Long crossing distance; Concerns about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Add curb extension at northeast corner of intersection	Medium	Medium	Yes
San Pedro Street and Winston Street					
Long crossing distance	Curb ramp/extension	Add curb extension at east side of existing crosswalk	Medium	Medium	Yes
Concerns about drivers not yielding to pedestrians at crossings	Crossing enhancement	Upgrade uncontrolled crosswalk with a Pedestrian Hybrid Beacon crossing	High	Medium	No
San Pedro Street and 5th Street					
Transit stop lacks a shade canopy	Transit	Install transit shelter	Medium	Long	Yes
Concerns about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Add curb extensions to the southeast and northeast corners of the intersection	High	Medium	Yes
Inadequate time to cross street	Signal modification	Review intersection for increased pedestrian crossing time across San Pedro Street	Low	Short	No
San Pedro Street, 4th Street to 6th Street					
Corridor lacks consistent pedestrian lighting	Lighting	Add pedestrian-scale lighting along corridor	High	Long	Yes
San Pedro Street, midblock, between 5th and 6th Street					
Long distance between controlled crossings	Crossing enhancement	Install mid-block crossing with Pedestrian Hybrid Beacon	High	Medium	Yes

Location						
Issue	Category	Recommendation	Cost Opinion*	Implementation Complexity (Short/Medium/Long Term)	External funding / coordination required	
San Pedro and 6th Street						
Inadequate crossing time		Study intersection for a pedestrian scramble phase	Low	Medium	No	
Concerns about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Install curb extension on northwest corner	Medium	Medium	Yes	
Inadequate time to cross intersection	Signal modification	Increase pedestrian crossing time across San Pedro Street	Low	Short	No	
Crocker Street and 4th Street						
Concerns about drivers not yielding to pedestrians at crossings	Crossing enhancement	Add high-visibility crosswalks across Crocker Street and Omar Street	Medium	Short	No	
Crocker Street and 5th Street						
Concerns about drivers not yielding to pedestrians at crossings	Crossing enhancement	Install Rapid Rectangular Flashing Beacon (RRFB) and high-visibility crosswalk across 5th Street	High	Medium	Yes	
Crocker Street, 5th Street to 6th Street						
Corridor lacks consistent pedestrian lighting	Lighting	Add pedestrian-scale lighting along block	Medium	Long	Yes	
Crocker Street and 6th Street						
Concerns about drivers not yielding to pedestrians at crossings	Crossing enhancement	Install Rapid Rectangular Flashing Beacon (RRFB) and high-visibility crosswalk across 6th Street	Medium	Medium	Yes	
Towne Avenue and 6th Street						
Bus stop lacks seating or shade	Transit	Install transit shelter on southwest corner of intersection	Medium	Long	No	
Main Street and 5th Street						
Bus stop lacks seating or shade	Transit	Install transit shelter on southeast corner of intersection	Medium	Long	No	

Location					
Issue	Category	Recommendation	Cost Opinion*	Implementation Complexity (Short/Medium/Long Term)	External funding / coordination required
5th Street, Main Street to Central Avenue					
Bike lane is frequently obstructed and does not provide adequate separation from vehicles	Bicycle	Upgrade bike lane with hardened physical separation materials	High	Medium	No
6th Street, Main Street to Alameda Street					
Bike lane is frequently obstructed and does not provide adequate separation from vehicles	Bicycle	Upgrade bike lane with hardened physical separation materials	High	Medium	No
5th Street and Wall Street					
Long crossing distance; Concerns about drivers not yielding to pedestrians at crossings	Curb ramp/extension	Add curb extension to southwest corner of intersection, extending into Wall Street	Medium	Medium	Yes
4th Street, Los Angeles Street to Towne Avenue					
Corridor lacks shade	Shade canopy	Add shade trees along corridor	High	Long	Yes
5th Street and San Julian Street					
Concerns about drivers not yielding to pedestrians	Crossing enhancement	Convert existing north-south crossing to a Pedestrian Hybrid Beacon	High	Medium	No
Narrow and obstructed sidewalk	Sidewalk	Expand sidewalk on east side of San Julian Street, south of intersection	Medium	Long	Yes
6th Street and Wall Street					
Inadequate crossing time	Signal modification	Increase pedestrian crossing time across 6th Street	Low	Short	No

*Cost opinions were developed based on sources available at the time of plan completion.



Chapter 7

Next Steps

Funding and Implementation

The Downtown neighborhood plan will support implementation by underpinning infrastructure grant applications. The document summarizes the comprehensive planning process that analyzed data, engaged the community, and produced project recommendations. Table 3 provides a list of potential grant funding opportunities for LADOT to pursue.

The infrastructure recommendations included in this Plan are within census tracts scoring between the 98th and 81st percentile of CalEnviroScreen 4.0 and are within disadvantaged communities under Senate Bill 535. These criteria are particularly relevant because many California funding opportunities prioritize projects that address environmental justice and equity, increasing the likelihood of securing grants for improvements in Downtown.

Older adults are essential members of the Downtown community. The ability to age in place and live safely, comfortably, and meaningfully in one's own home and community depends profoundly on the quality of the public realm. Safe crossings, shaded sidewalks, adequate lighting, and places to rest support autonomy and social participation. This plan provides a framework for building neighborhoods where aging in place is not only possible, but celebrated.

LADOT will continue to assess opportunities for implementation, coordinate across city departments, and pursue grants and partnerships to bring these improvements to life. Through these efforts, Los Angeles affirms its dedication to creating safer, more inclusive streets, ensuring that Angelenos can remain active, connected, and at home in their neighborhoods for years to come.

TABLE 4 Funding Opportunities

Funding Source	Available Funding and Timeline	Eligible SRFS Pilot Neighborhood Plan Recommendations
AARP Community Challenge Grant, AARP	In 2025, AARP provided \$4.2 million in funding across 383 grantees. Applications open annually.	Infrastructure recommendations in this Plan are eligible for Flagship Grant funding.
Active Transportation Program (ATP), California Transportation Commission (CTC)	In 2025, the CTC provided \$169 million in ATP funding. Applications open annually.	Infrastructure recommendations are eligible for Infrastructure Only Grants.
Local Highway Safety Improvement Program (HSIP), Caltrans	In 2025, Caltrans provided \$300 million in available funding. Calls for projects are made every two years.	Infrastructure recommendations are eligible for HSIP funding. The minimum grant amount is \$100,000, and the maximum grant amount is \$10 million. The majority of available funding goes to projects that have a Benefit to Cost Ratio of over 3.5.

Funding Source	
Available Funding and Timeline	Eligible SRFS Pilot Neighborhood Plan Recommendations
Metro Active Transport, Transit, and First/Last Mile (MAT) Program, Los Angeles Metro	\$857 million is available over the course of 40 years; \$75 million was available for Cycle 2 (implementation during FY2026-2030).
In 2024, SCAG provided \$8.2 million in available funding. Applications open annually.	Infrastructure recommendations within a ½ mile of the Little Tokyo/Arts District Metro station are eligible for FLM grants.
Safe Streets and Roads for All (SS4A) U.S. Department of Transportation	Infrastructure recommendations that require minor construction activity (e.g., does not require excavation) and uses durable, low-to-medium cost materials to pilot and iterate through project designs are eligible for Quick-Build Project funding. The maximum award per project is \$900,000.
Transformative Climate Communities, California Strategic Growth Council and Department of Conservation	\$5-\$6 billion is available between 2022 and 2026. Applications open annually.
In 2023, \$88.5 million was available for three Implementation Grant awards. Applications open annually.	Infrastructure recommendations on corridors identified in the city's Vision Zero Plan are eligible for Implementation Grant funding.
Better Utilizing Investments to Leverage Development (BUILD), U.S. Department of Transportation	Infrastructure recommendations are eligible and the SRFS project area meets funding requirements for an Implementation Grant (51 percent of project area must overlap with census tracts designated as disadvantaged). Multiple co-applicants are required.
\$1.5 billion available yearly. Applications open annually.	Infrastructure recommendations are eligible for BUILD grants.